



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Allyson Foster Boyd Allyson Foster Boyd
2026.02.04 11:49:40-05'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

CDM Smith
1100 Marion Street
Suite 300
Knoxville, TN 37921
Allyson Foster Boyd, P.E. No. 119,736

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES & UTILITY OWNERS	2
GENERAL NOTES AND SPECIAL NOTES.....	2C
TABULATED QUANTITIES	2F
GUARDRAIL PLAN AND FLUME DETAIL BR. NO. 47-10275-1.02 L&R	3
TRAFFIC CONTROL PLAN BR. NO. 47-10275-1.02 L&R WEST OLDHAM AVENUE/ WEST WOODLAND AVENUE PHASE 1	4, 4A - 4B
TRAFFIC CONTROL PLAN BR. NO. 47-10275-1.02 L&R WEST OLDHAM AVENUE/ WEST WOODLAND AVENUE PHASE 2.....	5, 5A - 5B
TRAFFIC CONTROL PLAN BR. NO. 47-10275-1.02 L&R I-275	6, 6A - 6H



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Susanne Dawson Susanne Dawson
2026.02.04 11:03:59-05'00'

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CDM Smith
1100 Marion Street
Suite 300
Knoxville, TN 37921
Susanne Dawson, P.E. No. 111,826

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS.....	1A
LAYOUT OF BRIDGES TO BE REPAIRED.....	BR-133-319
ESTIMATED BRIDGE QUANTITIES.....	BR-133-320
GENERAL NOTES.....	BR-133-321
SUPERSTRUCTURE REPAIRS	BR-133-322
ABUTMENT REPAIRS	BR-133-323
BENT 1 REPAIRS.....	BR-133-324
BENT 2 REPAIRS.....	BR-133-325
BENT 3 REPAIRS.....	BR-133-326
CONCRETE REPAIR DETAILS	BR-133-327
BEAM COLLISION REPAIR DETAILS.....	BR-133-328
BEAM END REPAIR DETAILS.....	BR-133-329

YEAR	CONST. NO.	SHEET NO.
2026	471275-M3-004	BRIDGE-SIGN1

KNOX CO. I-275

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Allyson Foster Boyd Allyson Foster Boyd
2026.04.06 16:32:43-04'00'

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CDM Smith
1100 Marion Street
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Allyson Foster Boyd, P.E. No. 119,736

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN2
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES & UTILITY OWNERS	2
TABULATED QUANTITIES	2F
TRAFFIC CONTROL PLAN BR. NO. 47-10275-1.02	6E, 6G, 6H



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

SUSANNE DAWSON SUSANNE DAWSON
2026.04.06 17:07:41-04'00'

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CDM Smith
1100 Marion Street
Suite 300
Knoxville, TN 37921
Susanne Dawson, P.E. No. 111,826

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN2
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS	1A
LAYOUT OF BRIDGES TO BE REPAIRED	BR-133-319
ESTIMATED BRIDGE QUANTITIES	BR-133-320
GENERAL NOTES	BR-133-321

YEAR	CONST. NO.	SHEET NO.
2026	471275-M3-004	BRIDGE-SIGN2

KNOX CO. I-275
REV. 03-24-2026: ADDED THIS SHEET TO PLANS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF ENGINEERING

KNOX COUNTY

I-275 BRIDGE
OVER OLDHAM AVENUE, LM 1.02

PS&E

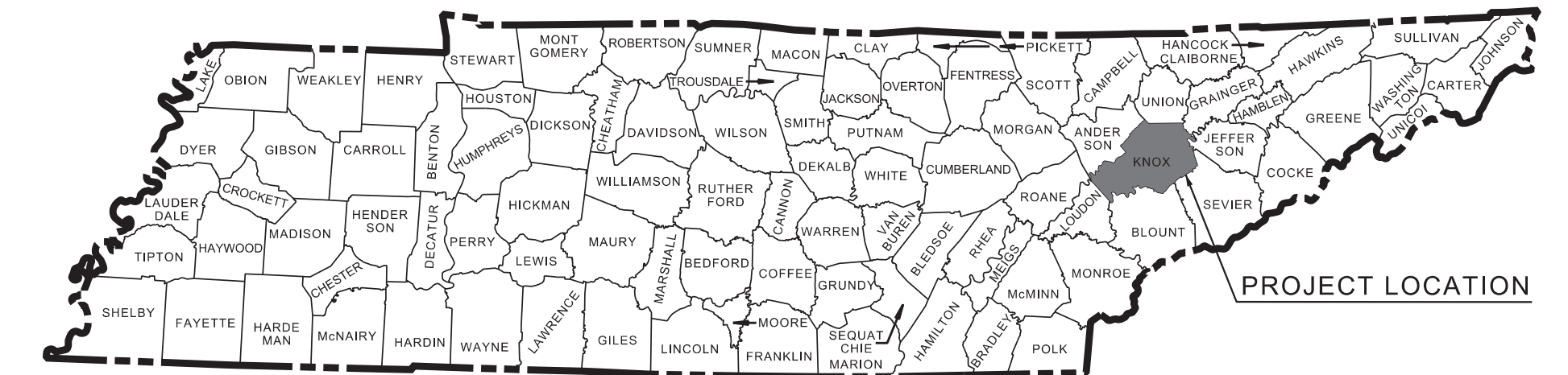
BRIDGE REPAIR

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-275

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	<input checked="" type="radio"/> NO
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	<input checked="" type="radio"/> YES	<input type="radio"/> NO

REV. 03-24-2026: UPDATED STANDARD SPECIFICATIONS AND FED. AID PROJ. NO.

TENN.	YEAR	SHEET NO.
	2026	1
FED. AID PROJ. NO.	N/A	
STATE PROJ. NO.	471275-M3-004	
FED. BRIDGE ID NOS.	47102750007 & 47102750008	



BRIDGE ID. NO. 47-10275-1.020



SCALE: 1"= 1 MILE

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED APRIL 1, 2026 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : STEPHEN WILSON

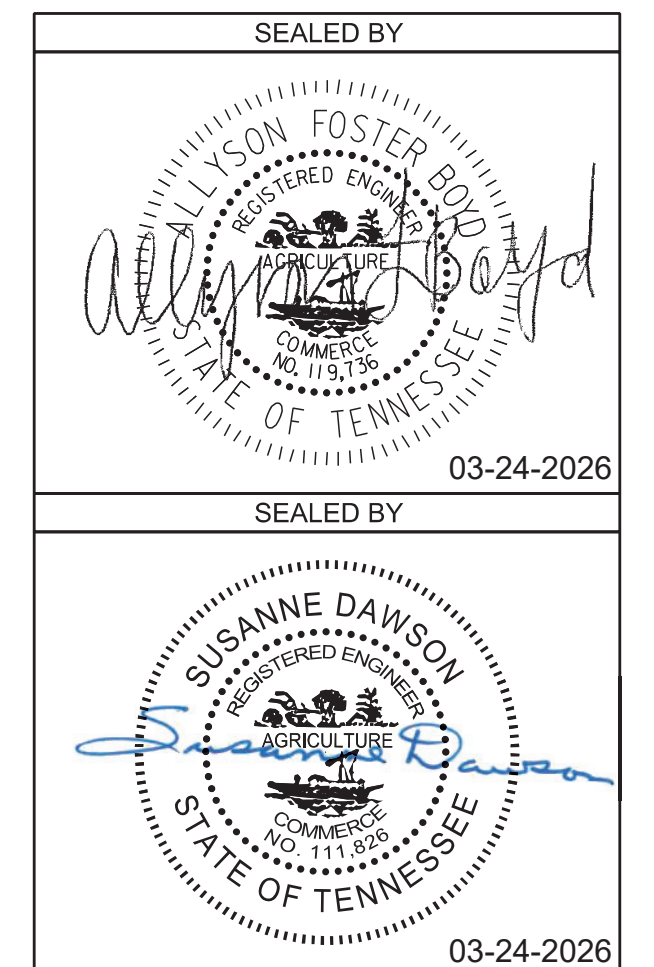
DESIGN FIRM : CDM Smith

DESIGNER : SUSANNE DAWSON / ALLYSON FOSTER BOYD CHECKED BY FRANK BALE / KASSIE HOLMES

P.E. NO. 471275-M3-004

PIN NO. 083980.01

TOTAL DISTURBED AREA = LESS THAN 0.95 ACRE



APPROVED: 
SHANE HESTER, CHIEF ENGINEER

DATE: _____

APPROVED: 
WILL REID, COMMISSIONER

I-275 OLDHAM AVE

TRAFFIC DATA		TRAFFIC DATA	
ADT (2024)	73,907	ADT (2024)	12,042
ADT (2044)	88,688	ADT (2044)	14,450
DHV (2024)	8,130	DHV (2024)	1,084
D	65 - 35	D	56 - 44
T (ADT)	29 %	T (ADT)	3 %
T (DHV)	29 %	T (DHV)	3 %
V	55 MPH	V	40 MPH

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	1A
PS&E	2026	471275-M3-004	1A

KNOX COUNTY I-275
REV. 03-24-2026: UPDATED ROADWAY INDEX, ADDED LIST OF REFERENCE DRAWINGS, UPDATED RD-L-1.

ROADWAY INDEX

SHEET NAME

SHEET NO.

SIGNATURE SHEET	BRIDGE-SIGN1
SIGNATURE SHEET	BRIDGE-SIGN2
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES & UTILITY OWNERS	2
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GUARDRAIL PLAN AND FLUME DETAIL BR. NO. 47-10275-1.02 L&R	3
TRAFFIC CONTROL PLAN BR. NO. 47-10275-1.02 L&R W OLDHAM AVE / W WOODLAND AVE PHASE 1	4, 4A – 4B
TRAFFIC CONTROL PLAN BR. NO. 47-10275-1.02 L&R W OLDHAM AVE / W WOODLAND AVE PHASE 2	5, 5A – 5B
TRAFFIC CONTROL PLAN BR. NO. 47-10275-1.02 L&R I-275.....	6, 6A – 6H

STANDARD ROADWAY DRAWINGS

DWG. REV. DESCRIPTION

STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

RD-A-1	02/20/2020	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	01/30/2026	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
RD-L-7	02/20/2020	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL

AQUATIC ORGANISM PASSAGE (AOP) DESIGN, PIPE CULVERTS, AND ENDWALLS

D-FLU-3	04/14/2025	BRIDGE END DRAIN FLUME DETAILS
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SAFETY DESIGN AND GUARDRAILS

S-CC-1	10/01/2024	CRASH CUSHION
S-AGT-1		LONG SPAN APPROACH GUARDRAIL TRANSITION
S-AGT-2		LONG SPAN APPROACH GUARDRAIL TRANSITION
S-AGT-3		LONG SPAN APPROACH GUARDRAIL TRANSITION
S-AGT-4		LONG SPAN APPROACH GUARDRAIL TRANSITION
S-AGT-5		LONG SPAN APPROACH GUARDRAIL TRANSITION
S-AGT-6		LONG SPAN APPROACH GUARDRAIL TRANSITION
S-AGT-7		LONG SPAN APPROACH GUARDRAIL TRANSITION
S-GR31-1	10/31/2025	GUARDRAIL DETAILS
S-GRT-2	06/28/2019	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2R	06/28/2019	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINALS (RETROFIT)

EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-19	04/01/2008	CATCH BASIN PROTECTION
EC-STR-39A	08/01/2012	CURB INLET PROTECTION TYPE 3 & 4

STANDARD TRAFFIC DESIGN DRAWINGS

DESIGN - TRAFFIC CONTROL

T-WZ-10	03/26/2025	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03/26/2025	ONE LANE CLOSURE DETAIL FOR DIVIDED HIGHWAYS
T-WZ-12	03/26/2025	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-22		ONE LANE CLOSURE DETAIL FOR UNDIVIDED HIGHWAYS
T-WZ-63	03/26/2025	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64	03/26/2025	WORK ZONE IN THE VICINITY OF AN EXIT RAMP
T-WZ-FAB1	03/26/2025	FLASHING YELLOW ARROW BOARD
T-WZ-PBR2	03/26/2025	DETAILS FOR WORK ZONE CHANNELIZATION DEVICES
T-WZ-PCB1	03/26/2025	10 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB2	03/26/2025	20 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB3	03/26/2025	PORTABLE CONCRETE BARRIER RAIL DETAILS
T-WZ-PCB4	07/22/2025	PORTABLE CONCRETE BARRIER RAIL ANCHOR PIN DETAILS

LIST OF BRIDGE DRAWINGS

DRAWING	DWG. NO.
LAYOUT OF BRIDGES TO BE REPAIRED	BR-133-319
ESTIMATED BRIDGE QUANTITIES	BR-133-320
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BEAM END REPAIR DETAILS	BR-133-329

LIST OF REFERENCE DRAWINGS

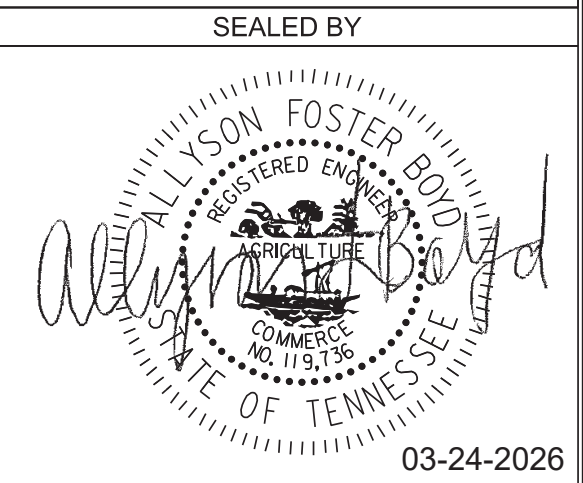
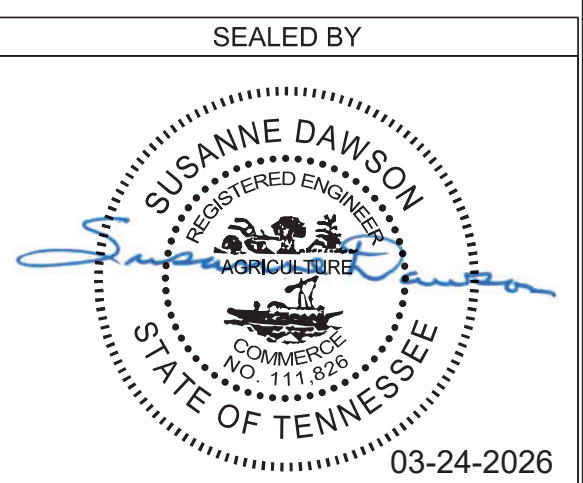
(TO BE PRINTED WITH PLANS)
PHASE II TMS LAYOUT-SHEET NO. 124

LIST OF BRIDGE REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)
F-11-9, F-11-45, F-11-45A THRU F-11-49
BR-27-91, BR-27-92, BR-27-94 THRU BR-27-96

LIST OF SPECIAL PROVISIONS

NO.	REV.	DESCRIPTION
604FRP		FIBER-REINFORCED POLYMER MATERIAL



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

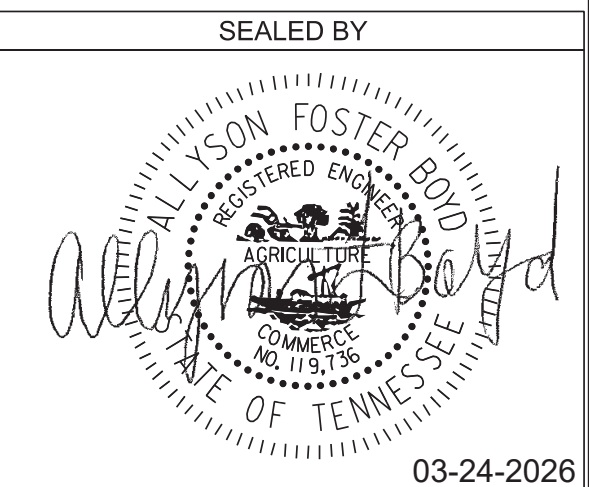
INDEX
AND
STANDARD
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	1B
PS&E	2026	471275-M3-004	1B

KNOX COUNTY I-275
REV. 03-24-2026: ADDED EDHZ003

PROJECT COMMITMENTS

COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	An Asbestos Containing Material (ACM) survey was completed on Bridge No. 47102750008 I-275 SB over Oldham Avenue LM 1.02 (47-10275-01.02L). The bridge has 16 asbestos deck drains at 15% Chrysotile and 2% Crocidolite. Please see the report for further details and photographs.	Bridge No. 47102750008 I-275 SB over Oldham Avenue, LM 1.02 (47-10275-01.02L)
EDHZ002	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	The State of Tennessee asbestos accreditation requirements (TDEC Rules Chapter 1200-01-20) mandates that ACM abatement work be performed by an accredited firm (contractor) using accredited abatement workers and supervisors. Abatement of this material shall be accomplished per SP202ACM Special Provision Regarding Removal of Asbestos-Containing Materials. ACM abatement should be completed prior to any demolition activities if possible. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	Bridge No. 47102750008 I-275 SB over Oldham Avenue, LM 1.02 (47-10275-01.02L)
EDHZ003	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	An Asbestos Containing Material (ACM) survey was completed on Bridge No. 47102750007 I-275 NB over Oldham Avenue LM 1.02 (47-10275-01.02R). The bridge has asbestos in 16 deck drains at 25% Chrysotile and 3% Crocidolite. Please see the report for further details and photographs. See note EDHZ002.	Bridge No. 47102750007 I-275 NB over Oldham Avenue LM 1.02 (47-10275-01.02R)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**PROJECT
COMMITMENTS**

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	2A
PS&E	2026	471275-M3-004	2

KNOX COUNTY I-275
 REV. 03-24-2026: ADDED ITEM 713-15.02 AND FOOTNOTE 13.
 ADDED ITEM 712-09.04. REVISED ITEM 712-06.

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
9	203-01 ROAD & DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	274
10	209-09.43 CURB INLET PROTECTION (TYPE 4)	EACH	6
10	209-40.30 CATCH BASIN PROTECTION (TYPE A)	EACH	3
	705-04.09 EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH	2
	705-06.01 W BEAM GR (TYPE 2) MASH TL-3	L.F.	250
	705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH	2
12	705-06.28 THRIE BEAM AGT (LONG SPAN) MASH TL-3	EACH	2
	706-01 GUARDRAIL REMOVED	L.F.	383
1	709-05.06 MACHINED RIP-RAP (CLASS A-1)	TON	479
2	712-01 TRAFFIC CONTROL	LS	1
3 4	712-02.10 PORTABLE BARRIER RAIL (MASH TL-3)	L.F.	2,240
5	712-02.60 TEMPORARY WORK ZONE CRASH CUSHION (MASH TL-3)	EACH	2
2	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	298
	712-04.50 BARRIER RAIL DELINEATOR	EACH	112
	712-05.01 WARNING LIGHTS (TYPE A)	EACH	22
6	712-06 SIGNS (CONSTRUCTION)	S.F.	996
	712-07.02 TEMPORARY BARRICADES (TYPE II)	L.F.	24
	712-07.03 TEMPORARY BARRICADES (TYPE III)	L.F.	96
	712-08.03 ARROW BOARD (TYPE C)	EACH	3
7	712-09.02 REMOVABLE PAVEMENT MARKING (8" BARRIER LINE)	L.F.	7,631
7	712-09.04 REMOVABLE PAVEMENT MARKING (ARROW)	EACH	1
7	712-09.08 REMOVABLE PAVEMENT MARKING (6" LINE)	L.F.	5,219
11	712-09.21 REMOVABLE WET REFLECTIVE PAVEMENT MARKING TAPE	L.F.	436
7	712-09.31 REMOVABLE BLACK-OUT TAPE (8")	L.F.	4,515
13	713-15.02 REMOVAL AND RELOCATION OF SIGN	EACH	1
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	7
	717-01 MOBILIZATION	LS	1
8	740-10.03 GEOTEXTILE (TYPE III) (EROSION CONTROL)	S.Y.	669

FOOTNOTES

- INCLUDES 467 TONS FOR FLUME AND 12 TONS FOR GULLY REPAIR. THIS ITEM SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR CONSTRUCTION.
- ALL COSTS ASSOCIATED WITH INSTALLING, STORING, AND RE-INSTALLING ALL TRAFFIC CONTROL DEVICES DURING AND BETWEEN THE DIFFERENT TRAFFIC CONTROL PHASES WILL BE INCLUDED IN THE BID ITEM OF EACH ITEM. DURING THE TIME BETWEEN DIFFERENT TRAFFIC CONTROL PHASES, THE CONTRACTOR SHALL STORE ALL TRAFFIC CONTROL DEVICES IN A PROPER LOCATION THAT WILL NOT INTERFERE WITH THE TRAFFIC FLOW. ALL WORK MUST MEET THE FULL APPROVAL OF THE TDOT ENGINEER.
- ITEM SHALL INCLUDE INSTALLING AND RELOCATING PORTABLE BARRIER RAIL.
- ITEM MAY BE REPLACED WITH MASH TL-3 APPROVED BARRIER FROM QPL 45, IF UNAVAILABLE. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS LISTED FOR THE APPROVED BARRIER.
- THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS LISTED ON THE MANUFACTURER'S SHOP DRAWING.
- THIS ITEM INCLUDES THE COST OF INSTALLATION, INSPECTION AND MAINTENANCE OF ANY SIGN SHEETING AND SUPPORTS AND TRAFFIC CONTROL APPURTENANCES DURING THE CONSTRUCTION OPERATIONS.
- TO SUPPORT LANE SHIFTS AND CHANGES TO TRAVEL PATH FOR THE LANE CLOSURES REQUIRED FOR CONSTRUCTION. ITEM INCLUDES APPLICATION AND REMOVAL OF TEMPORARY MARKINGS FOR EACH CONSTRUCTION PHASE.
- INCLUDES 629 SY FOR FLUMES AND 40 SY FOR GULLY REPAIR. THIS ITEM SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR CONSTRUCTION.
- INCLUDES 267 CY FOR FLUME AND 7 CY FOR GULLY REPAIR. THIS ITEM SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR CONSTRUCTION.
- SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT. ALL QUANTITIES ARE TO BE USED AS DIRECTED BY THE TDOT ENGINEER.
- FOR TEMPORARY CROSS-WALK TO SUPPORT PEDESTRIAN DETOUR ON OLDDHAM AVENUE.
- ITEM INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR GROUTING EXISTING HOLES IN WINGPOSTS AND DRILLING HOLES FOR NEW GUARDRAIL ATTACHMENT.
- RELOCATE SIGN AND POST FOR W1-2L WITH W13-1 PLAQUE TO SUPPORT FLUME PLACEMENT.

UTILITY OWNERS

GAS:

KNOXVILLE UTILITIES BOARD
 P.O. BOX 59017
 4505 MIDDLEBROOK PIKE
 KNOXVILLE, TN 37921-5599
 CONTACT: TIM KELLEY; AARON BOTT
 OFFICE PHONE: (865) 558-2577
 CELL PHONE: (865) 320-2681; (423) 341-2565
 EMAIL: timothy.kelly@kub.org; aaron.bott@kub.org

TELEPHONE/ COMMUNICATIONS:

AT&T
 9733 PARKSIDE DRIVE
 KNOXVILLE, TN 37922
 CONTACT: JAY FRAZIER
 CELL PHONE: (865) 387-2685
 EMAIL: jf092g@att.com

WATER, SEWER:

KNOXVILLE UTILITIES BOARD
 P.O. BOX 59017
 4505 MIDDLEBROOK PIKE
 KNOXVILLE, TN 37921-5599
 CONTACT: NOAH NEWPORT
 OFFICE PHONE: (865) 558-2747
 CELL PHONE: (423) 494-0774
 EMAIL: noah.newport@kub.org

CABLE:

COMCAST/ XFINITY
 5720 ASHEVILLE HWY
 KNOXVILLE, TN 37924
 CONTACT: JAMES MCCAWLEY
 OFFICE PHONE: (865) 862-5061
 CELL PHONE: (865) 312-2340
 EMAIL: james_mccawley@cable.comcast.com

FIBER:

KNOXVILLE UTILITIES BOARD
 P.O. BOX 59017
 4505 MIDDLEBROOK PIKE
 KNOXVILLE, TN 37921-5599
 CONTACT: JAMES WAGNER
 OFFICE PHONE: (865) 558-2407
 CELL PHONE: (865) 789-3756
 EMAIL: james.wagner@kub.org

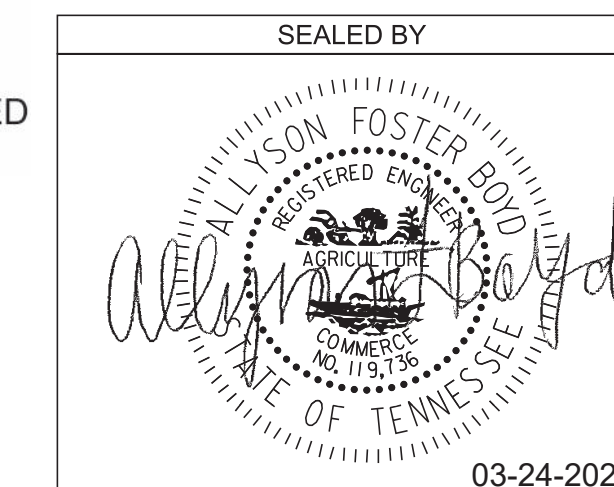
- NO UTILITY CONFLICTS ARE ANTICIPATED BASED ON THE SCOPE OF WORK.
- THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT", THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

LIGHTING:

CITY OF KNOXVILLE
 3131 MORRIS AVE
 KNOXVILLE, TN 37909
 CONTACT: CHEVELLE LEWIS
 OFFICE PHONE: (865) 215-6046
 CELL PHONE: (865) 659-5876
 EMAIL: clewis@knoxvilletn.gov
 streetlighting@knoxvilletn.gov

TRAFFIC SIGNALS:

CITY OF KNOXVILLE
 1400 LORAIN STREET
 KNOXVILLE, TN 37921
 OFFICE PHONE: (865) 215-6100



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

**ESTIMATED
 ROADWAY
 QUANTITIES &
 UTILITY OWNERS**

GENERAL NOTES

DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (1) BEFORE OPENING THE ROADWAY TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 712-09.02 REMOVABLE PAVEMENT MARKING (8" BARRIER LINE) PER L.F OR 712-09.08 REMOVABLE PAVEMENT MARKING (6" LINE) WHERE NOTED ON PLANS. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE COVERED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. COVERING OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01, TRAFFIC CONTROL, LUMP SUM.

TRAFFIC CONTROL DIRECTIONAL SIGNING

- (1) ON ALL ACCESS CONTROLLED AND INTERSTATE RECONSTRUCTION AND NEW CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL UTILIZE ALL EXISTING DIRECTIONAL SIGNING FOR AS LONG AS POSSIBLE. THESE EXISTING SIGNS CAN BE MOVED USING TEMPORARY SUPPORTS AS NEEDED. AS SOON AS THESE EXISTING DIRECTIONAL SIGNS COME DOWN PERMANENTLY, THE CONTRACTOR SHALL HAVE UP AT LEAST ONE NEW TEMPORARY "ADVANCE GUIDE SIGN" AND ONE NEW TEMPORARY "EXIT DIRECTIONAL SIGN" AT ALL EXIT RAMP. THESE SIGNS ARE TO BE MAINTAINED WITHIN CLEAR VIEW OF THE PUBLIC ON THE RIGHT SIDE OF THE HIGHWAY AND SHALL BE REPLACED IF DAMAGED, DURING ALL PHASES OF CONSTRUCTION, AS DIRECTED BY THE ENGINEER.
- (2) THE SIZE OF THESE NEW TEMPORARY SIGNS WILL BE DETERMINED BY THE MESSAGE. THE MESSAGE SHALL BE THE SAME AS THE EXISTING SIGN THAT THESE NEW TEMPORARY SIGNS WILL BE REPLACING. THE LETTER SIZE SHALL BE A MINIMUM OF 8 INCH, "D" UPPER CASE LETTER. THE DIRECTIONAL ARROW WILL BE A "B" ARROW AT A 45 DEGREE ANGLE (SAME ANGLE AS THE EXISTING ARROW). THE MATERIAL SHALL BE 0.100 INCH SHEET ALUMINUM; THE COLOR SHALL BE A REFLECTIVE GREEN BACKGROUND WITH REFLECTIVE WHITE COPY.
- (3) ALL WORK AND MATERIAL TO MAKE THESE NEW TEMPORARY DIRECTIONAL SIGNS ALONG WITH ADEQUATE SUPPORTS AND TO MOVE THEM AS NEEDED DURING EACH PHASE OF CONSTRUCTION WILL BE PAID FOR UNDER ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F. , AS DIRECTED BY THE ENGINEER.
- (4) SOME OF THESE DIRECTIONAL SIGNS WILL NEED AN INTERSTATE, U.S., OR A STATE HIGHWAY SHIELD, A CARDINAL DIRECTION, AND A DIRECTION ARROW TO ACCOMPANY THE DIRECTIONAL SIGN. THESE SIGNS SHALL BE MOUNTED BELOW THE DIRECTIONAL SIGN.
- (5) ALL EXISTING "EMERGENCY REFERENCE MARKERS" AND "HOSPITAL SIGNS" SHALL BE MAINTAINED WITHIN FULL VIEW OF THE MOTORING PUBLIC THROUGHOUT ALL PHASES OF CONSTRUCTION. ALL WORK IN MOVING AND TEMPORARY SUPPORTS SHALL BE PAID FOR UNDER ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F. .

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45)

FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8) ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

SPECIAL NOTES

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

ENVIRONMENTAL GENERAL NOTES

PROJECT COMMITMENTS

- (1) SEE PROJECT COMMITMENTS, SHEET 1B FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (1) THIS PROJECT IS A BRIDGE REPAIR PROJECT TO RESTORE THE I-275 BRIDGE OVER WEST OLDHAM AVENUE THAT IS IN NEED OF REPAIRS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

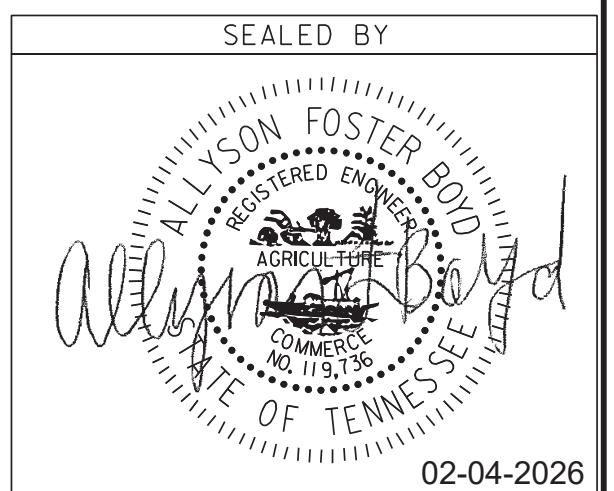
- (2) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (3) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (4) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (5) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (6) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (7) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (8) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (9) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (10) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (11) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (12) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (13) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	2B
PS&E	2026	471275-M3-004	2C

KNOX COUNTY I-275



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES
AND
SPECIAL NOTES**

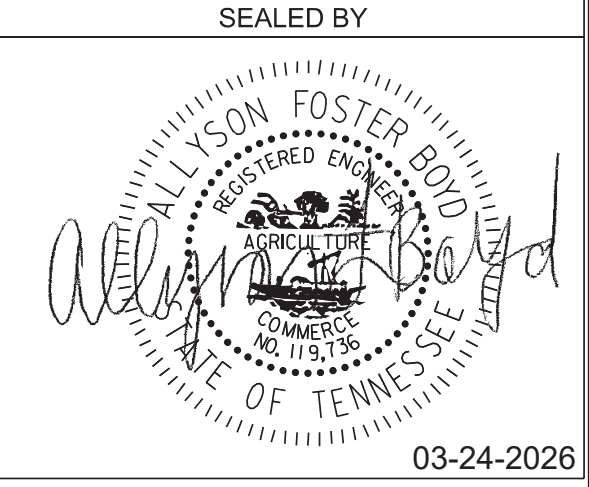
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	2A1
PS&E	2026	471275-M3-004	2F

KNOX COUNTY I-275
REV. 03-24-2026; REVISED I-275 SIGN QUANTITIES.

WEST OLDHAM AVE / WEST WOODLAND AVE TRAFFIC CONTROL SIGN QUANTITIES					
SIGN NO.	DESCRIPTION	SIZE	QUANTITY		AREA (S.F.)
			PHASE 1	PHASE 2	
G20-2	END ROAD WORK	36X18	5	5	22.5
G20-2	END ROAD WORK	48X24	2	2	16
R9-9	SIDEWALK CLOSED	24X12	2	2	4
R9-11L	SIDEWALK CLOSED AHEAD CROSS HERE	24X18	1	1	3
R9-11R	SIDEWALK CLOSED AHEAD CROSS HERE	24X18	1	1	3
R10-15L	ONE DIRECTION SYMBOL - LEFT	30X30	1	1	6.25
W1-4L	REVERSE CURVE SYMBOL - LEFT	36X36	1	1	9
W1-4R	REVERSE CURVE SYMBOL - RIGHT	36X36	1	1	9
W1-6R	ONE DIRECTION SYMBOL - RIGHT	36X36	1	0	9
W4-2L	LANE CLOSED MERGE LEFT	36X36	0	1	9
W4-2R	LANE CLOSED MERGE RIGHT	36X36	1	0	9
W11-2	PEDESTRIAN SYMBOL	36X36	8	8	72
W16-7P	DIAGONAL ARROW DOWN - SUPPLEMENTAL	21X15	4	4	8.75
W16-9P	AHEAD - SUPPLEMENTAL	24X12	4	4	8
W20-1	ROAD WORK AHEAD	36X36	5	5	45
W20-1	ROAD WORK AHEAD	48X48	2	2	32
CUSTOM	LEFT LANE CLOSED AHEAD	36X36	0	1	9
CUSTOM	RIGHT LANE CLOSED AHEAD	36X36	1	0	9
TOTAL					283.5

I-275 TRAFFIC CONTROL SIGN QUANTITIES				
SIGN NO.	DESCRIPTION	SIZE	QUANTITY	AREA (S.F.)
E5-1a	GORE EXIT WITH NUMBER	78X60	1	32.5
E5-2	EXIT OPEN	48X36	1	12
G20-2	END ROAD WORK	48X24	6	48
M1-1	INTERSTATE - 3 DIGITS	45X36	2	22.5
TN-44	WORKERS PRESENT REDUCE SPEED	78X60	2	65
W4-2R	LANE CLOSED MERGE RIGHT	48X48	4	64
W20-1	ROAD WORK AHEAD	48X48	1	16
W20-1	ROAD WORK 1/2 MILE	48X48	4	64
W20-1	ROAD WORK 1 MILE	48X48	8	128
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48X48	6	96
W20-5R	RIGHT LANE CLOSED 1500 FT	48X48	4	64
CUSTOM	TO WOODLAND/OLDHAM 45-RIGHT	48X60	1	20
CUSTOM	TO WOODLAND/OLDHAM STRAIGHT	48X60	1	20
CUSTOM	TO I-275 WOODLAND/OLDHAM RIGHT	48X60	1	20
CUSTOM	TO I-275 WOODLAND/OLDHAM STRAIGHT	48X60	2	40
TOTAL				712.0

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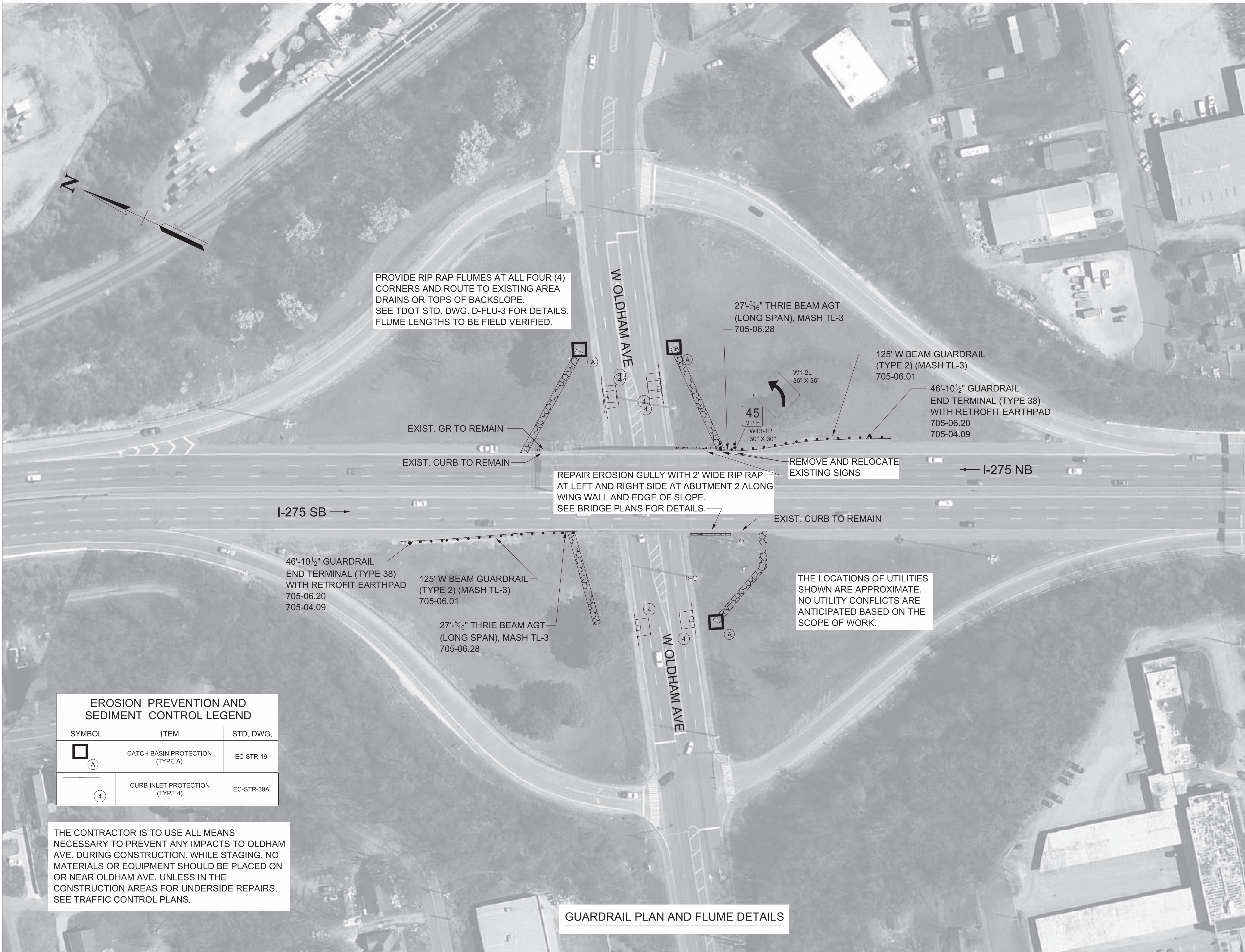


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**TABULATED
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	3
PS&E	2026	471275-M3-004	3

KNOX COUNTY I-275



PROVIDE RIP RAP FLUMES AT ALL FOUR (4) CORNERS AND ROUTE TO EXISTING AREA DRAINS OR TOPS OF BACKSLOPE. SEE TDOT STD. DWG. D-FLU-3 FOR DETAILS. FLUME LENGTHS TO BE FIELD VERIFIED.

27'-5/16" THRIE BEAM AGT (LONG SPAN), MASH TL-3 705-06.28

125' W BEAM GUARDRAIL (TYPE 2) (MASH TL-3) 705-06.01

46'-10 1/2" GUARDRAIL END TERMINAL (TYPE 38) WITH RETROFIT EARTH PAD 705-06.20 705-04.09

REPAIR EROSION GULLY WITH 2' WIDE RIP RAP AT LEFT AND RIGHT SIDE AT ABUTMENT 2 ALONG WING WALL AND EDGE OF SLOPE. SEE BRIDGE PLANS FOR DETAILS.

THE LOCATIONS OF UTILITIES SHOWN ARE APPROXIMATE. NO UTILITY CONFLICTS ARE ANTICIPATED BASED ON THE SCOPE OF WORK.

EROSION PREVENTION AND SEDIMENT CONTROL LEGEND

SYMBOL	ITEM	STD. DWG.
	CATCH BASIN PROTECTION (TYPE A)	EC-STR-19
	CURB INLET PROTECTION (TYPE 4)	EC-STR-39A

THE CONTRACTOR IS TO USE ALL MEANS NECESSARY TO PREVENT ANY IMPACTS TO OLDHAM AVE. DURING CONSTRUCTION. WHILE STAGING, NO MATERIALS OR EQUIPMENT SHOULD BE PLACED ON OR NEAR OLDHAM AVE. UNLESS IN THE CONSTRUCTION AREAS FOR UNDERSIDE REPAIRS. SEE TRAFFIC CONTROL PLANS.

GUARDRAIL PLAN AND FLUME DETAILS

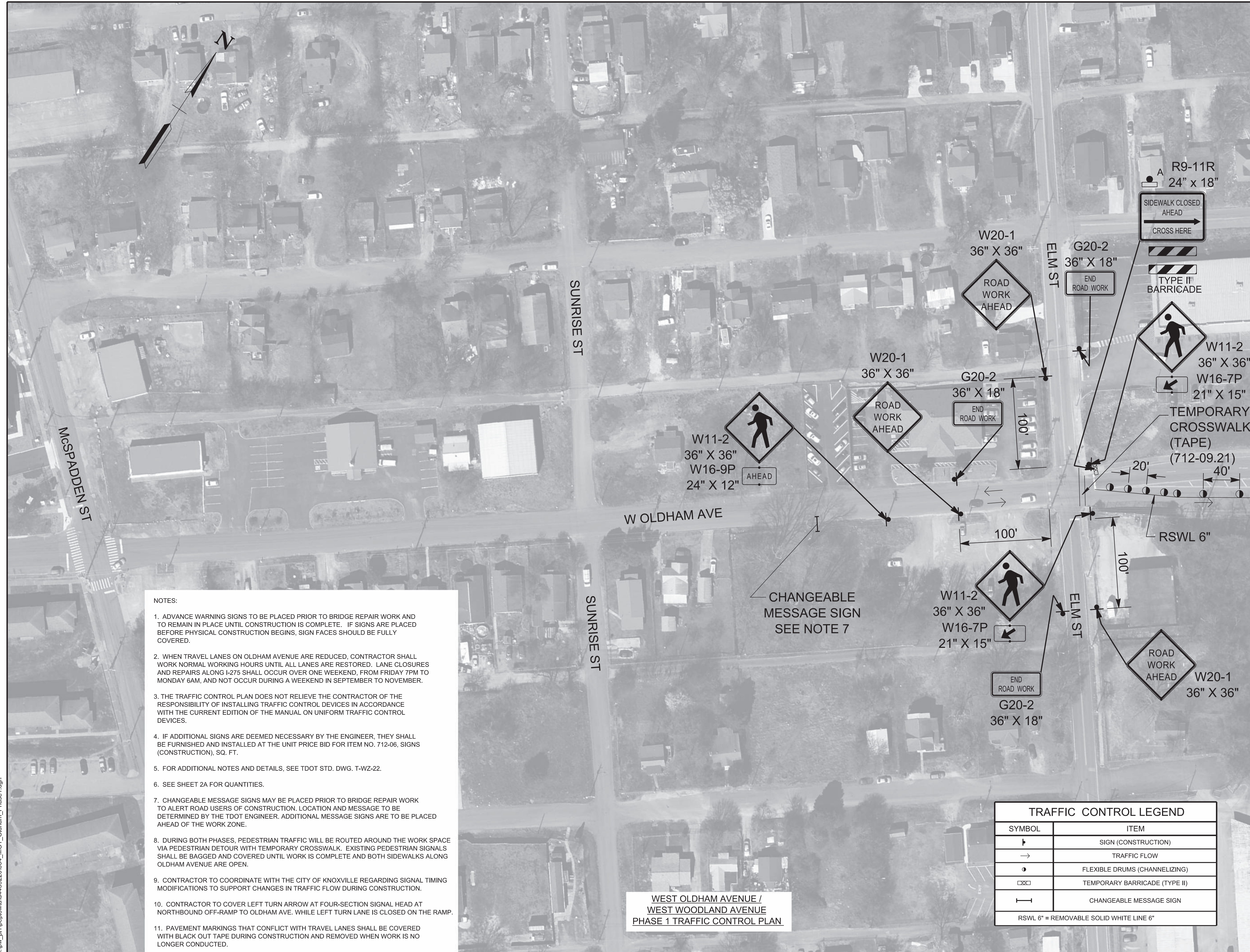
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GUARDRAIL PLAN & FLUME DETAIL
BR. NO. 47-10275-1.02 L&R

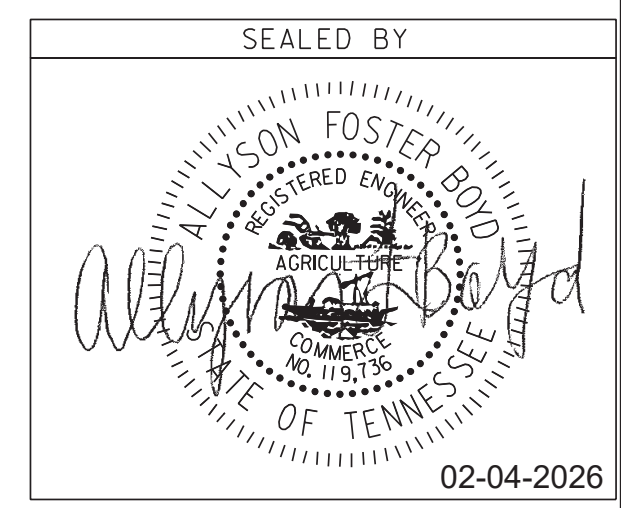
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	4
PS&E	2026	471275-M3-004	4
KNOX COUNTY			I-275



- NOTES:
1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 2. WHEN TRAVEL LANES ON OLDHAM AVENUE ARE REDUCED, CONTRACTOR SHALL WORK NORMAL WORKING HOURS UNTIL ALL LANES ARE RESTORED. LANE CLOSURES AND REPAIRS ALONG I-275 SHALL OCCUR OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
 3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-22.
 6. SEE SHEET 2A FOR QUANTITIES.
 7. CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
 8. DURING BOTH PHASES, PEDESTRIAN TRAFFIC WILL BE ROUTED AROUND THE WORK SPACE VIA PEDESTRIAN DETOUR WITH TEMPORARY CROSSWALK. EXISTING PEDESTRIAN SIGNALS SHALL BE BAGGED AND COVERED UNTIL WORK IS COMPLETE AND BOTH SIDEWALKS ALONG OLDHAM AVENUE ARE OPEN.
 9. CONTRACTOR TO COORDINATE WITH THE CITY OF KNOXVILLE REGARDING SIGNAL TIMING MODIFICATIONS TO SUPPORT CHANGES IN TRAFFIC FLOW DURING CONSTRUCTION.
 10. CONTRACTOR TO COVER LEFT TURN ARROW AT FOUR-SECTION SIGNAL HEAD AT NORTHBOUND OFF-RAMP TO OLDHAM AVE. WHILE LEFT TURN LANE IS CLOSED ON THE RAMP.
 11. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 1 TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬇	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
▢	TEMPORARY BARRICADE (TYPE II)
— —	CHANGEABLE MESSAGE SIGN
RSWL 6" = REMOVABLE SOLID WHITE LINE 6"	



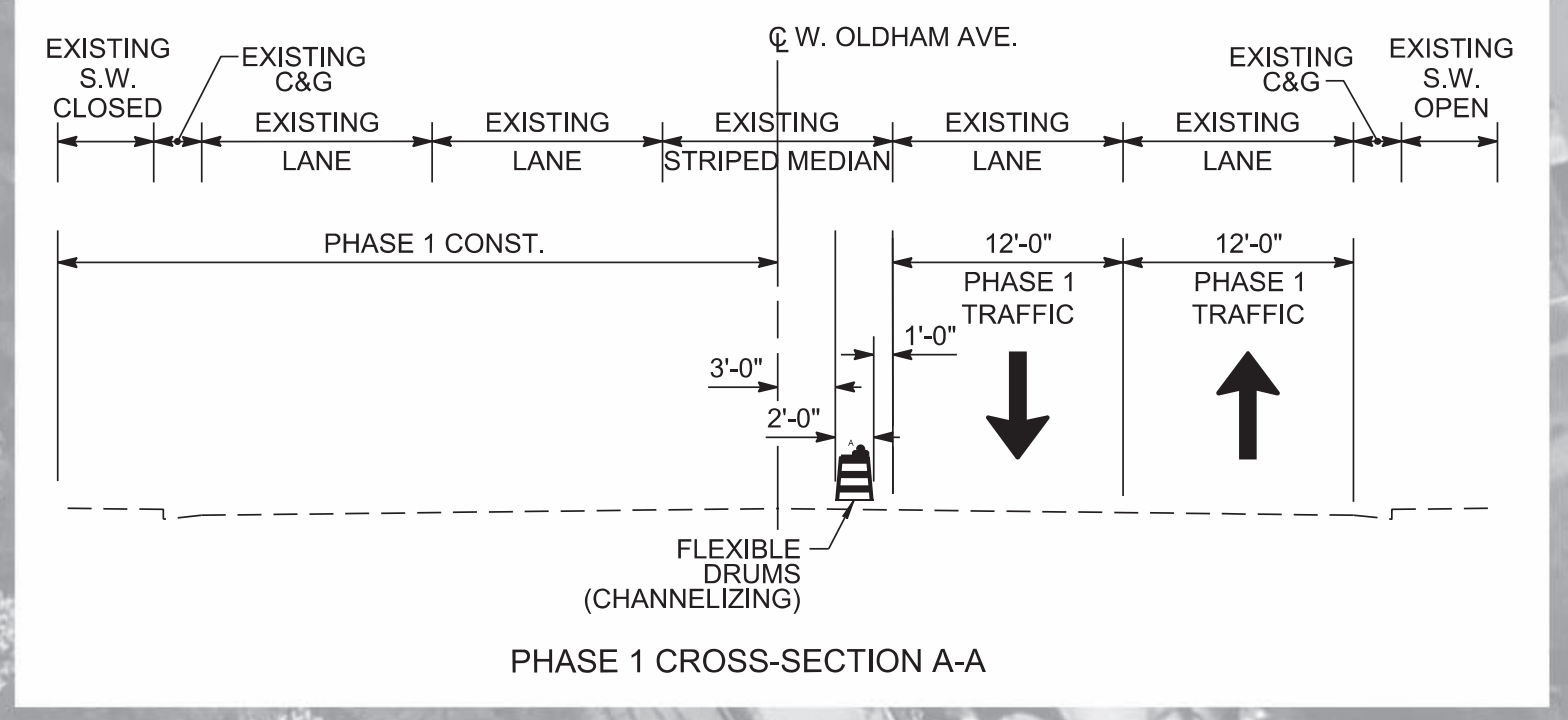
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 1

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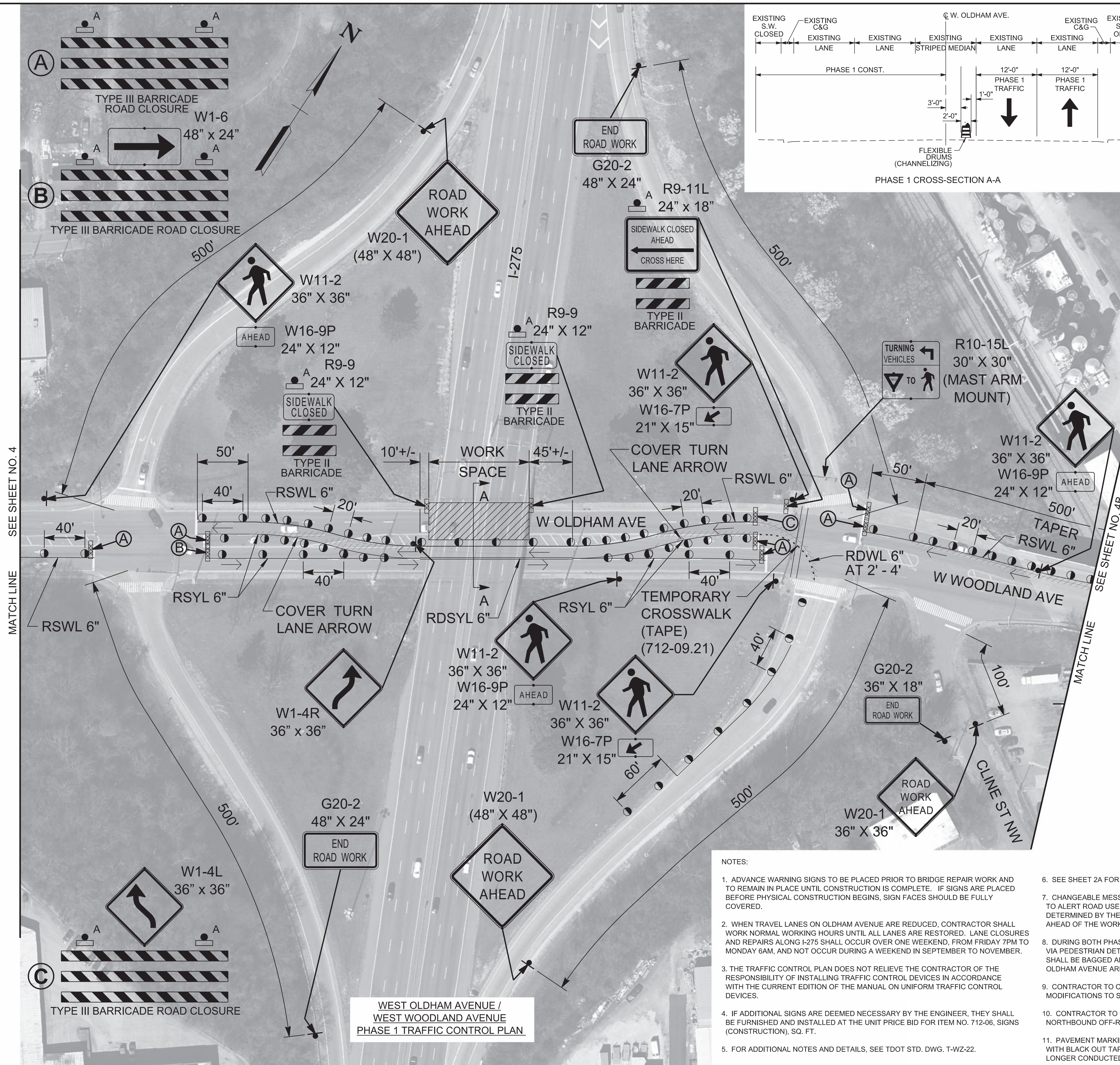
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	4A
PS&E	2026	471275-M3-004	4A

KNOX COUNTY I-275



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
▶	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////	REMOVABLE BLACK-OUT TAPE (8")
▢	TEMPORARY BARRICADE (TYPE III)
▢	TEMPORARY BARRICADE (TYPE II)
— —	CHANGEABLE MESSAGE SIGN

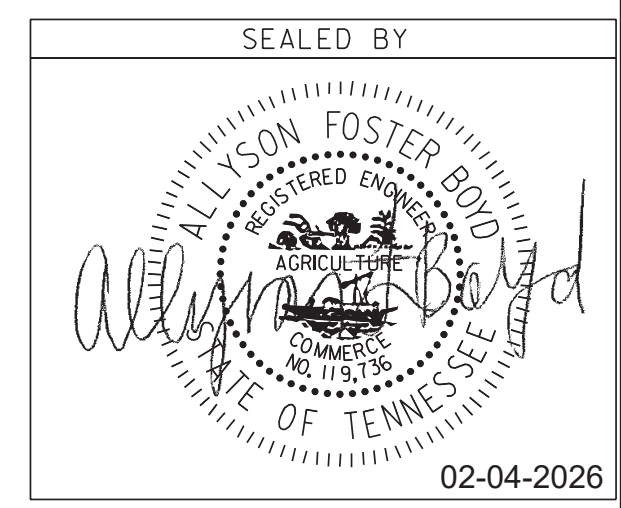
RDSYL 6" = REMOVABLE DOUBLE SOLID YELLOW LINE 6"
 RSWL 6" = REMOVABLE SOLID WHITE LINE 6"
 RSYL 6" = REMOVABLE SOLID YELLOW LINE 6"



NOTES:

- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
- WHEN TRAVEL LANES ON OLDHAM AVENUE ARE REDUCED, CONTRACTOR SHALL WORK NORMAL WORKING HOURS UNTIL ALL LANES ARE RESTORED. LANE CLOSURES AND REPAIRS ALONG I-275 SHALL OCCUR OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
- THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
- FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-W2-22.
- SEE SHEET 2A FOR QUANTITIES.
- CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
- DURING BOTH PHASES, PEDESTRIAN TRAFFIC WILL BE ROUTED AROUND THE WORK SPACE VIA PEDESTRIAN DETOUR WITH TEMPORARY CROSSWALK. EXISTING PEDESTRIAN SIGNALS SHALL BE BAGGED AND COVERED UNTIL WORK IS COMPLETE AND BOTH SIDEWALKS ALONG OLDHAM AVENUE ARE OPEN.
- CONTRACTOR TO COORDINATE WITH THE CITY OF KNOXVILLE REGARDING SIGNAL TIMING MODIFICATIONS TO SUPPORT CHANGES IN TRAFFIC FLOW DURING CONSTRUCTION.
- CONTRACTOR TO COVER LEFT TURN ARROW AT FOUR-SECTION SIGNAL HEAD AT NORTHBOUND OFF-RAMP TO OLDHAM AVE. WHILE LEFT TURN LANE IS CLOSED ON THE RAMP.
- PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

WEST OLDHAM AVENUE / WEST WOODLAND AVENUE PHASE 1 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10275-1.02 L&R
 WEST OLDHAM AVENUE / WEST WOODLAND AVENUE
 PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	4B
PS&E	2026	471275-M3-004	4B

KNOX COUNTY I-275

NOTES:

- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
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MATCH LINE SEE SHEET NO. 4A

500'
TAPER

20'

RSWL 6"

115'+/-

100'

100'

100'

CHANGEABLE MESSAGE SIGN SEE NOTE 7

W OLDHAM AVE

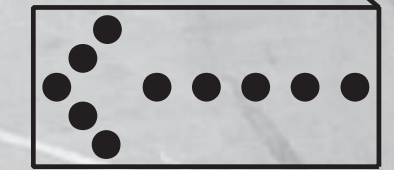
BANNER ST

W WOODLAND AVE

BANNER ST

END ROAD WORK

G20-2
36" X 18"



ARROW BOARD
(96"X48")



W4-2R
(36" X 36")



W20-5R
36" X 36"

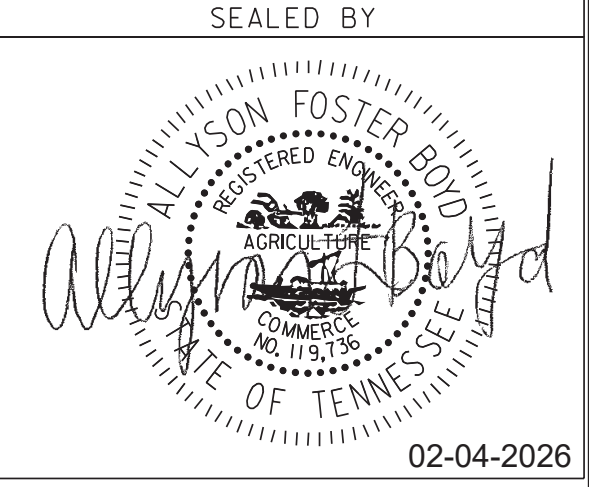


W20-1
36" X 36"

DAVANNA AVE

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬇	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
•	FLEXIBLE DRUMS (CHANNELIZING)
////	REMOVABLE BLACK-OUT TAPE (8")
— —	CHANGEABLE MESSAGE SIGN
RSWL 6" = REMOVABLE SOLID WHITE LINE 6"	

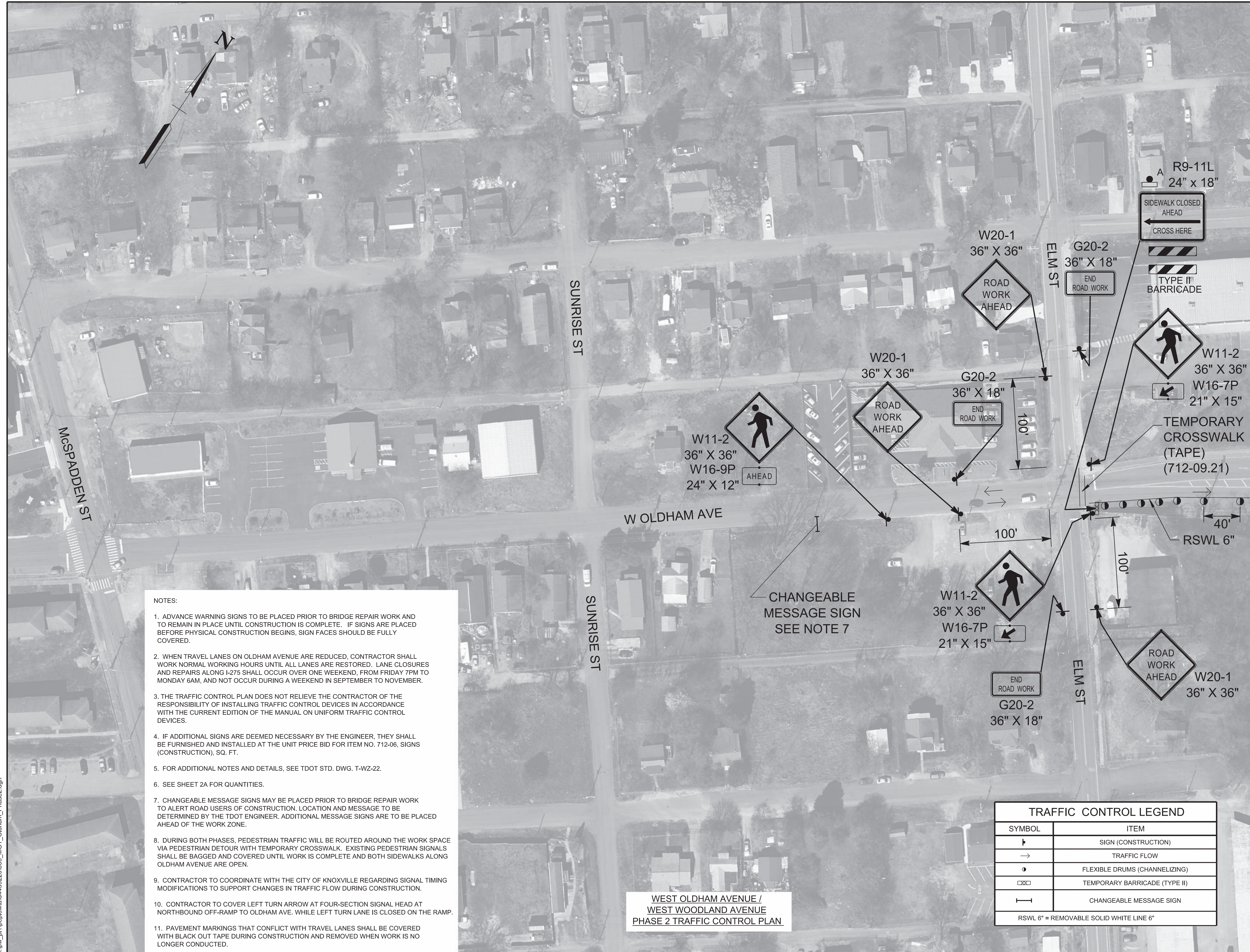
WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 1 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 1

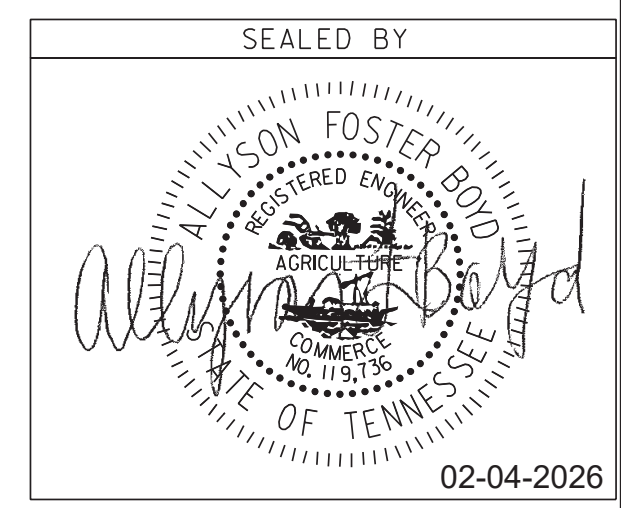
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	5
PS&E	2026	471275-M3-004	5
KNOX COUNTY			I-275



- NOTES:
1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 2. WHEN TRAVEL LANES ON OLDHAM AVENUE ARE REDUCED, CONTRACTOR SHALL WORK NORMAL WORKING HOURS UNTIL ALL LANES ARE RESTORED. LANE CLOSURES AND REPAIRS ALONG I-275 SHALL OCCUR OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
 3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
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 6. SEE SHEET 2A FOR QUANTITIES.
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 10. CONTRACTOR TO COVER LEFT TURN ARROW AT FOUR-SECTION SIGNAL HEAD AT NORTHBOUND OFF-RAMP TO OLDHAM AVE. WHILE LEFT TURN LANE IS CLOSED ON THE RAMP.
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WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 2 TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬇	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
▢	TEMPORARY BARRICADE (TYPE II)
— —	CHANGEABLE MESSAGE SIGN
RSWL 6" = REMOVABLE SOLID WHITE LINE 6"	

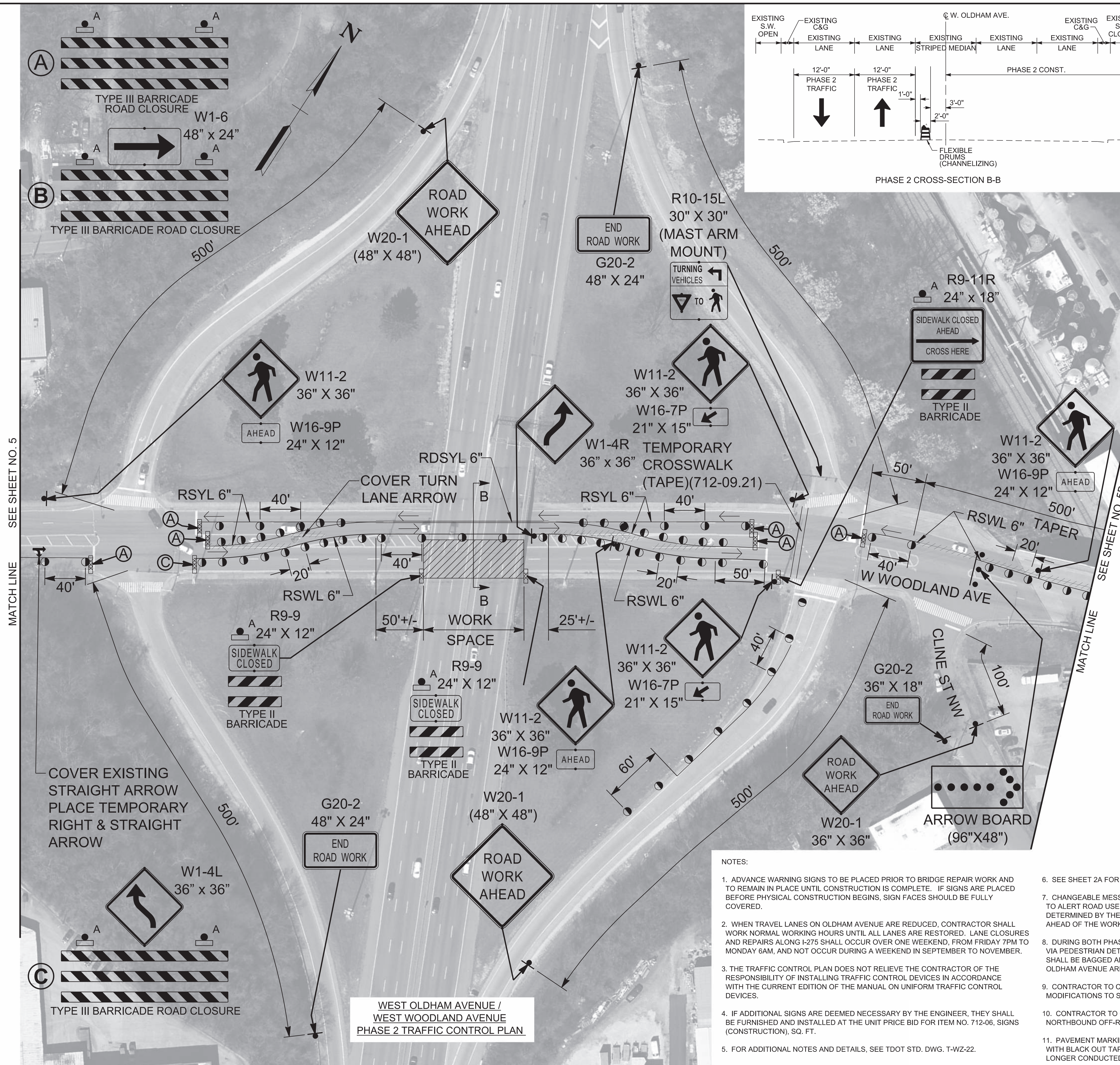
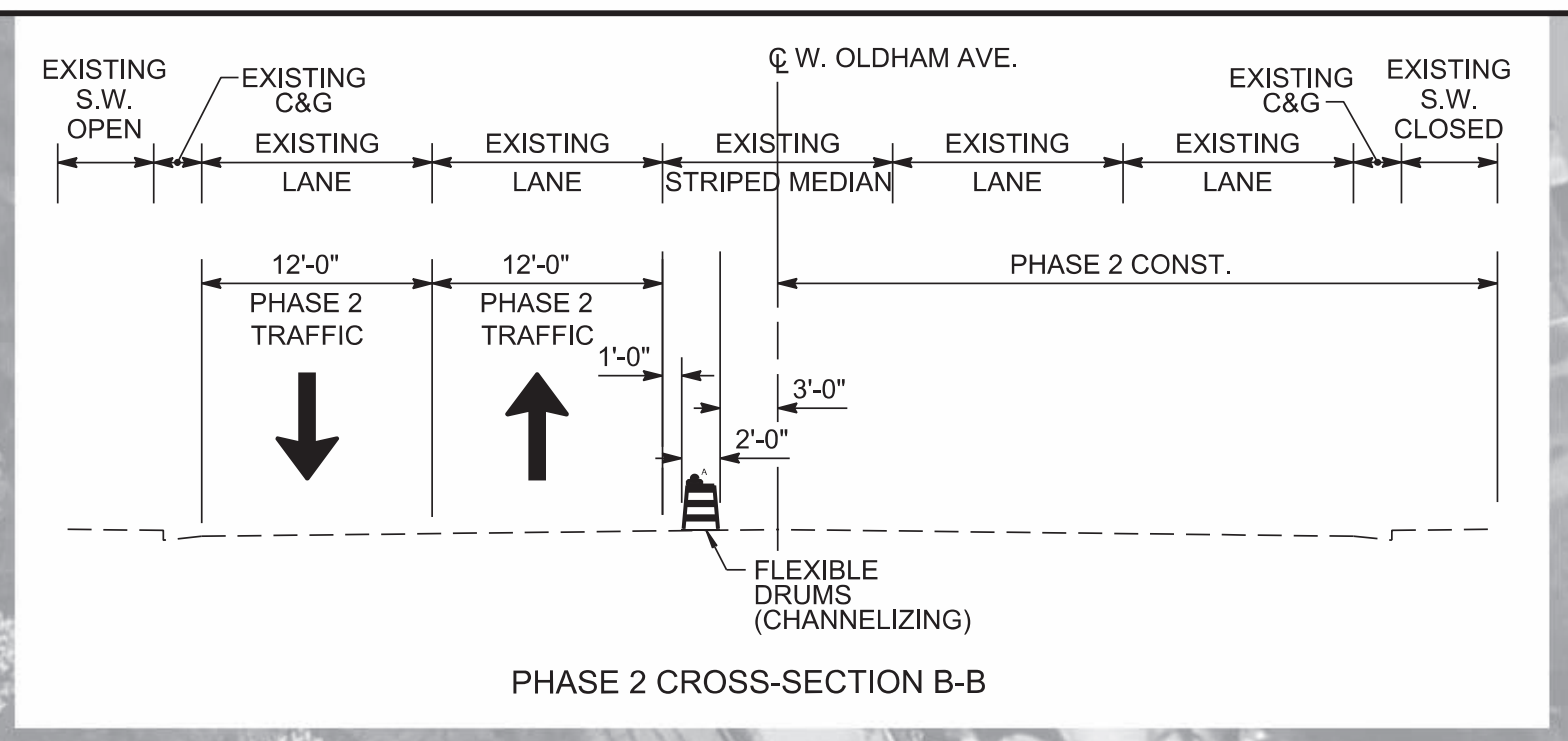


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 2

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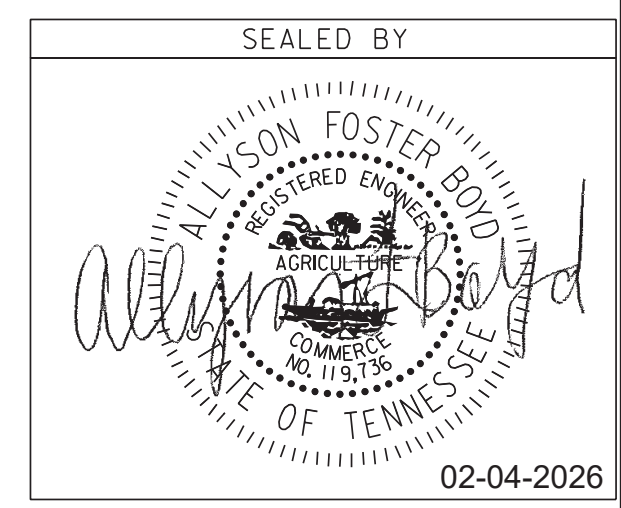
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	5A
PS&E	2026	471275-M3-004	5A
KNOX COUNTY			I-275



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
▶	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////	REMOVABLE BLACK-OUT TAPE (8')
▢	TEMPORARY BARRICADE (TYPE III)
▢	TEMPORARY BARRICADE (TYPE II)
— —	CHANGEABLE MESSAGE SIGN

RDSYL 6" = REMOVABLE DOUBLE SOLID YELLOW LINE 6"
 RSWL 6" = REMOVABLE SOLID WHITE LINE 6"
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STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10275-1.02 L&R
 WEST OLDHAM AVENUE /
 WEST WOODLAND AVENUE
 PHASE 2

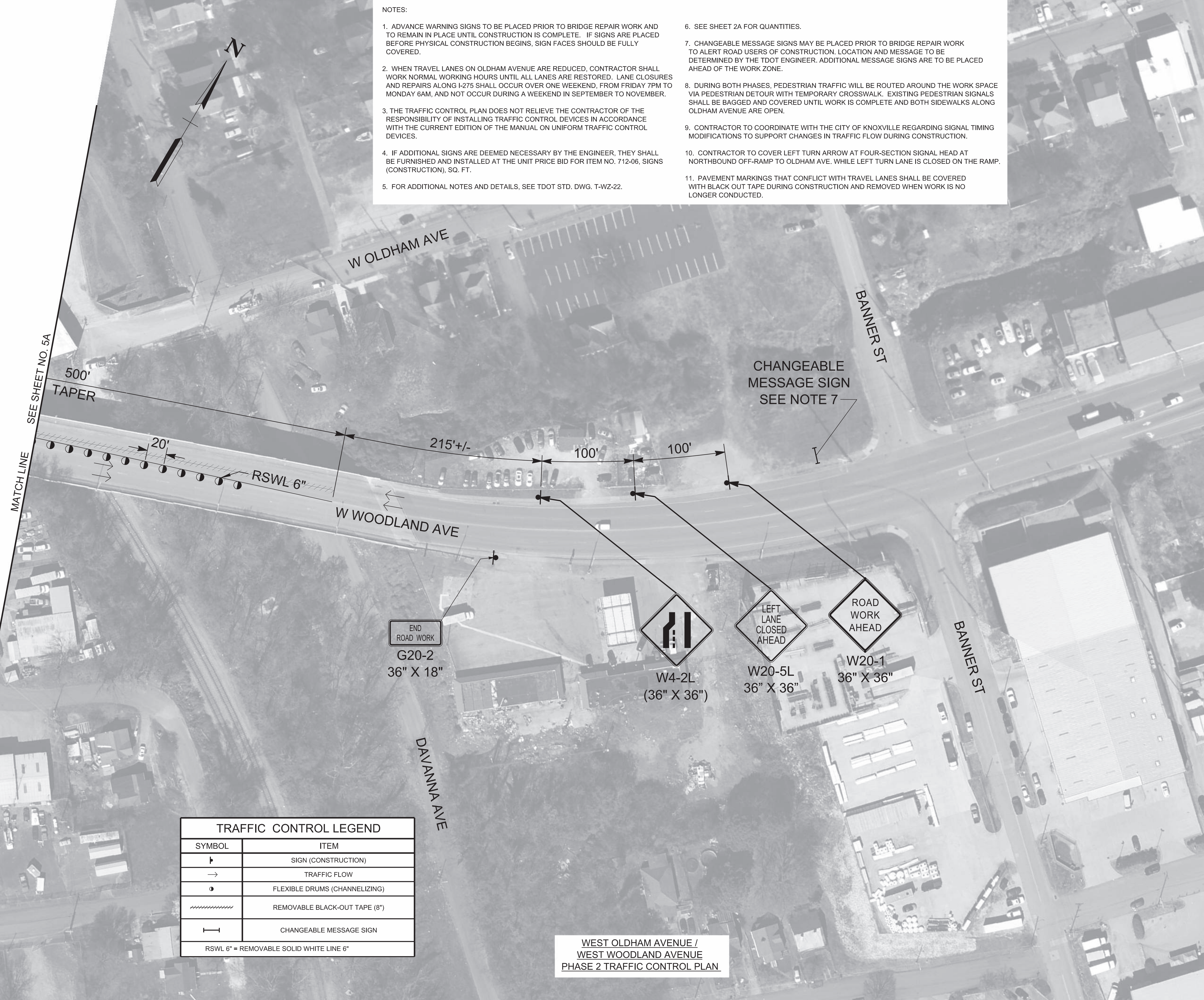
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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	5B
PS&E	2026	471275-M3-004	5B

KNOX COUNTY I-275

NOTES:

- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
▬	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////	REMOVABLE BLACK-OUT TAPE (8")
— —	CHANGEABLE MESSAGE SIGN
RSWL 6" = REMOVABLE SOLID WHITE LINE 6"	

WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

02-04-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
WEST OLDHAM AVENUE /
WEST WOODLAND AVENUE
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	6
PS&E	2026	471275-M3-004	6

KNOX COUNTY I-275

- NOTES:
1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 2. WHEN TRAVEL LANES ON I-275 ARE REDUCED, CONTRACTOR SHALL CONDUCT REPAIRS OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT TO OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
 3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
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 5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-11, T-WZ-12, T-WZ-63 AND T-WZ-64.
 6. SEE SHEET 2A FOR QUANTITIES.
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* W20-1 (ROAD WORK 1 MILE) SIGNS TO BE PLACED AT EB AND WB I-640 RAMP MERGE AND AT I-275 AND CLINTON HWY RAMP MERGE



HEISKELL AVE

I-275

VARIABLE

1300'

1300'

2600'

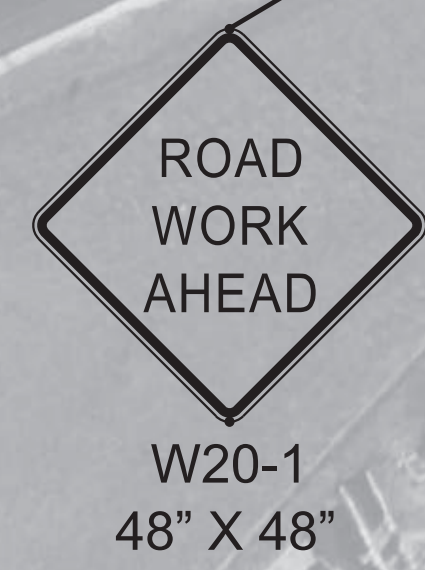
1100'

500'

500'

1000'

CHANGEABLE MESSAGE SIGN SEE NOTE 7



I-275 TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
▬	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
•••••	ARROW BOARD TYPE C (SINGLE ARROW)

MATCH LINE SEE SHEET NO. 6A

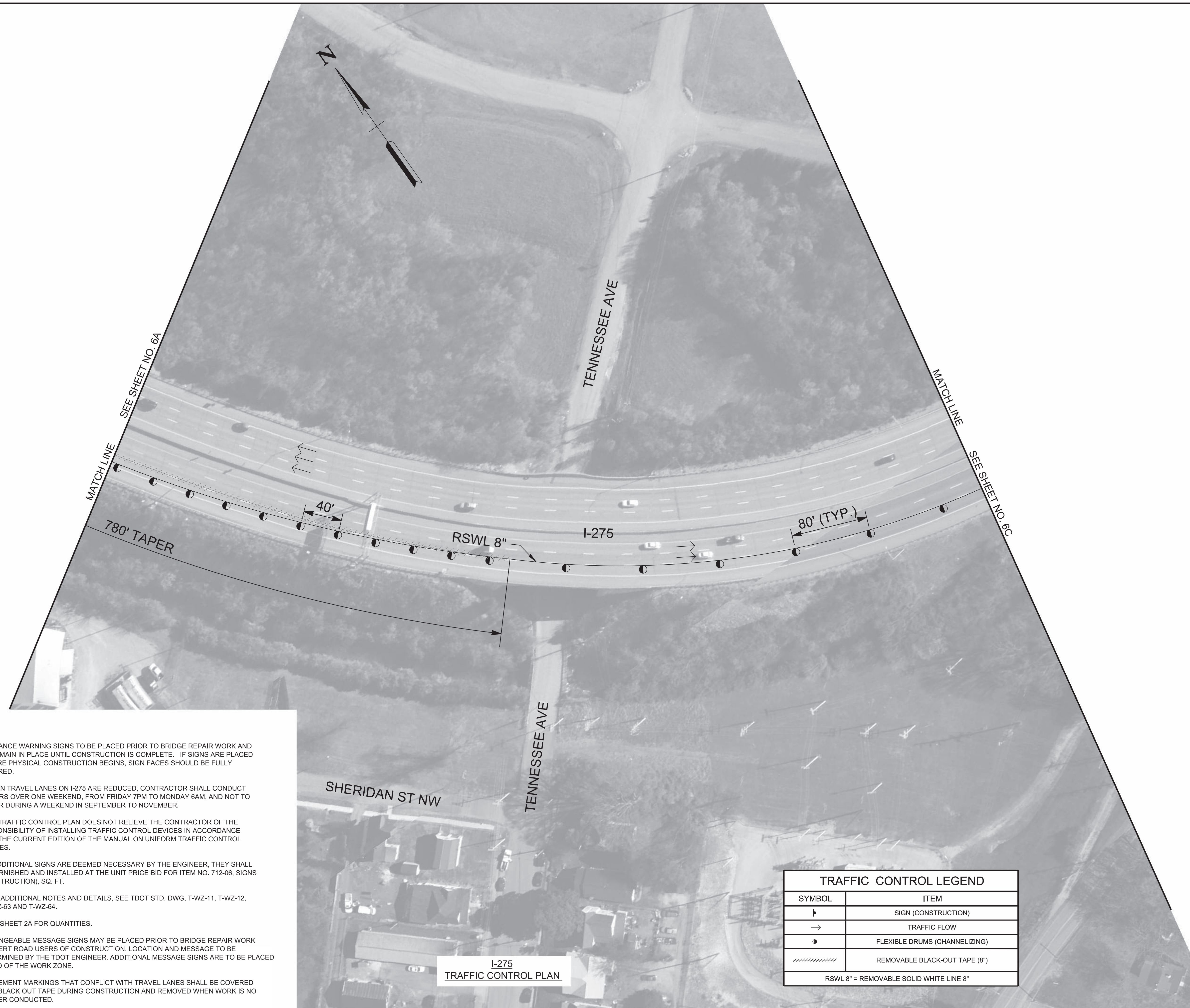
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
I-275

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	6B
PS&E	2026	471275-M3-004	6B

KNOX COUNTY I-275



NOTES:

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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬇	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////	REMOVABLE BLACK-OUT TAPE (8')
RSWL 8" = REMOVABLE SOLID WHITE LINE 8"	

SEALED BY

02-04-2026

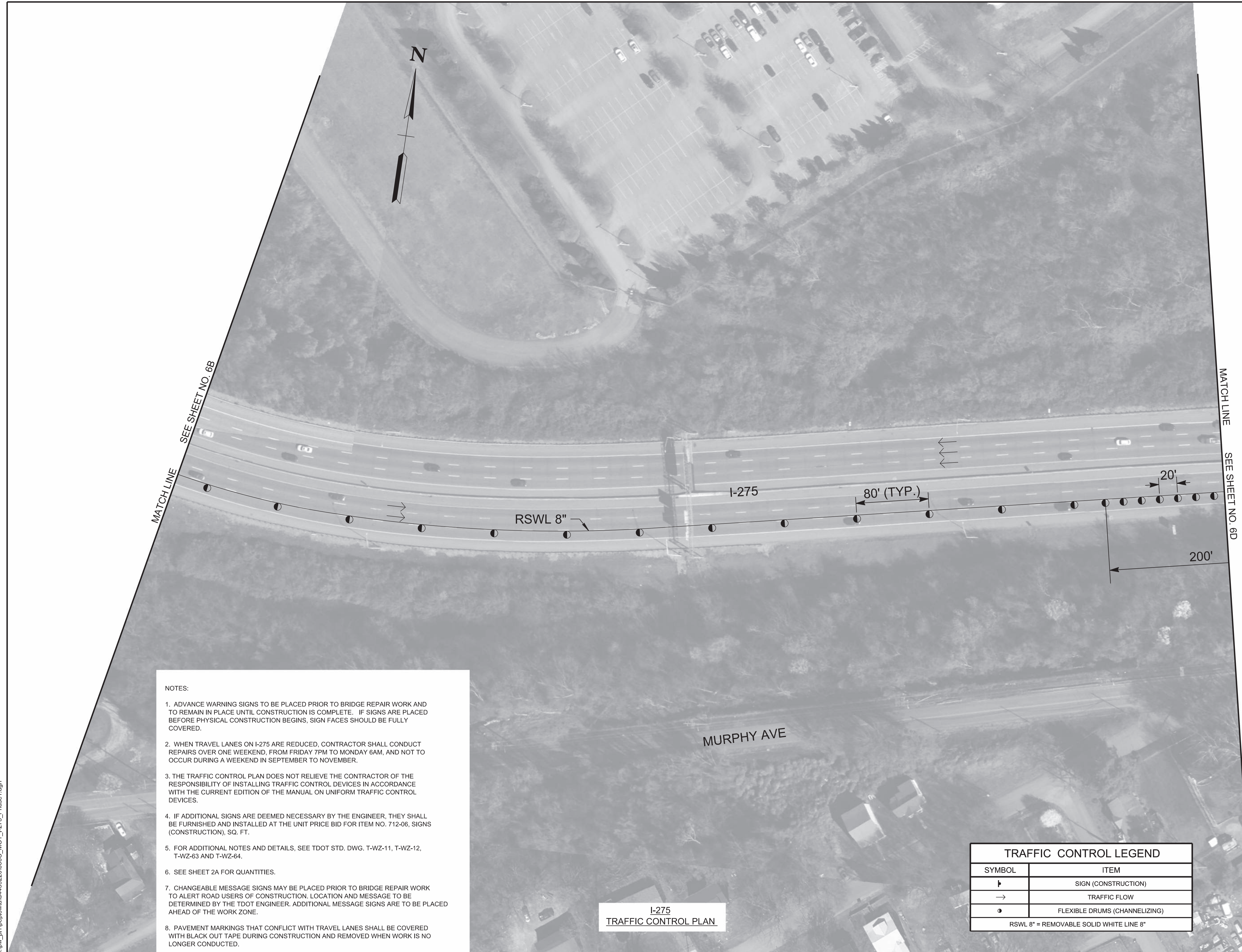
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
I-275

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	6C
PS&E	2026	471275-M3-004	6C

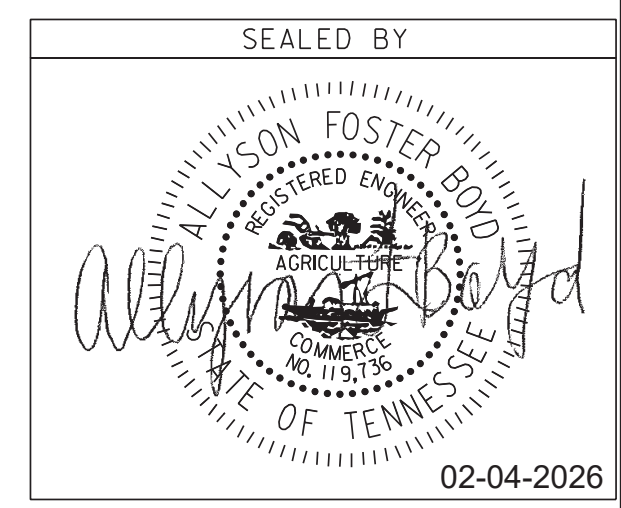
KNOX COUNTY I-275



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1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 2. WHEN TRAVEL LANES ON I-275 ARE REDUCED, CONTRACTOR SHALL CONDUCT REPAIRS OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT TO OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
 3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-11, T-WZ-12, T-WZ-63 AND T-WZ-64.
 6. SEE SHEET 2A FOR QUANTITIES.
 7. CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
 8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

I-275
TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
▬	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
RSWL 8" = REMOVABLE SOLID WHITE LINE 8"	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
I-275

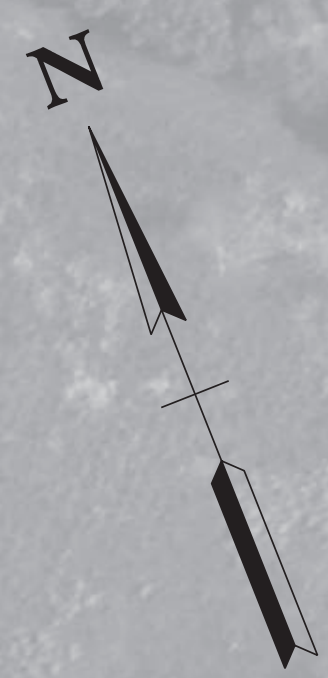
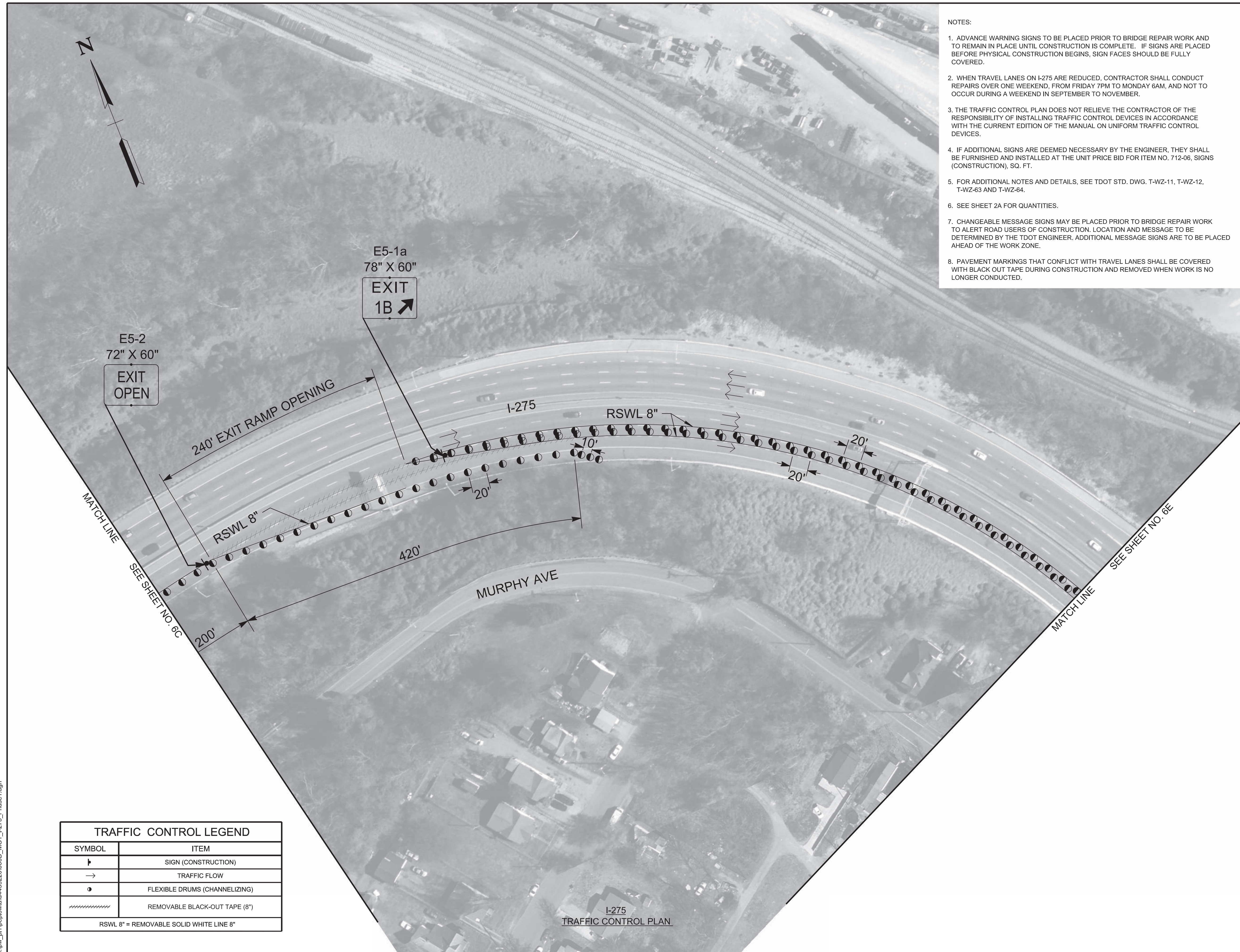
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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	6D
PS&E	2026	471275-M3-004	6D

KNOX COUNTY I-275

NOTES:

1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
2. WHEN TRAVEL LANES ON I-275 ARE REDUCED, CONTRACTOR SHALL CONDUCT REPAIRS OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT TO OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-11, T-WZ-12, T-WZ-63 AND T-WZ-64.
6. SEE SHEET 2A FOR QUANTITIES.
7. CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.



E5-2
72" X 60"
EXIT
OPEN

E5-1a
78" X 60"
EXIT
1B

MATCH LINE

SEE SHEET NO. 6C

240' EXIT RAMP OPENING

RSWL 8"

420'

MURPHY AVE

I-275

RSWL 8"

10'

20'

20'

20'

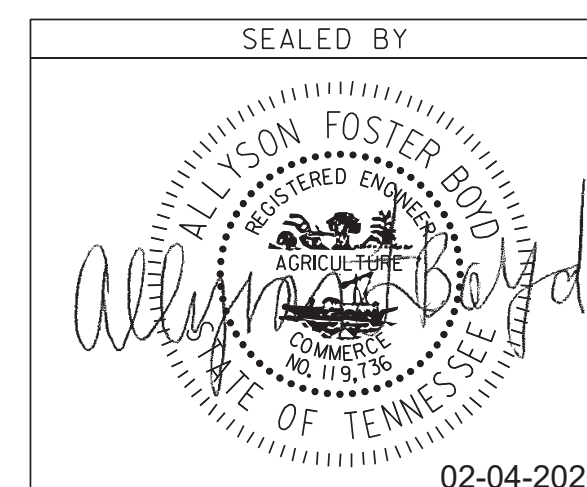
MATCH LINE

SEE SHEET NO. 6E

I-275

TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
↑	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////	REMOVABLE BLACK-OUT TAPE (8")
RSWL 8" = REMOVABLE SOLID WHITE LINE 8"	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

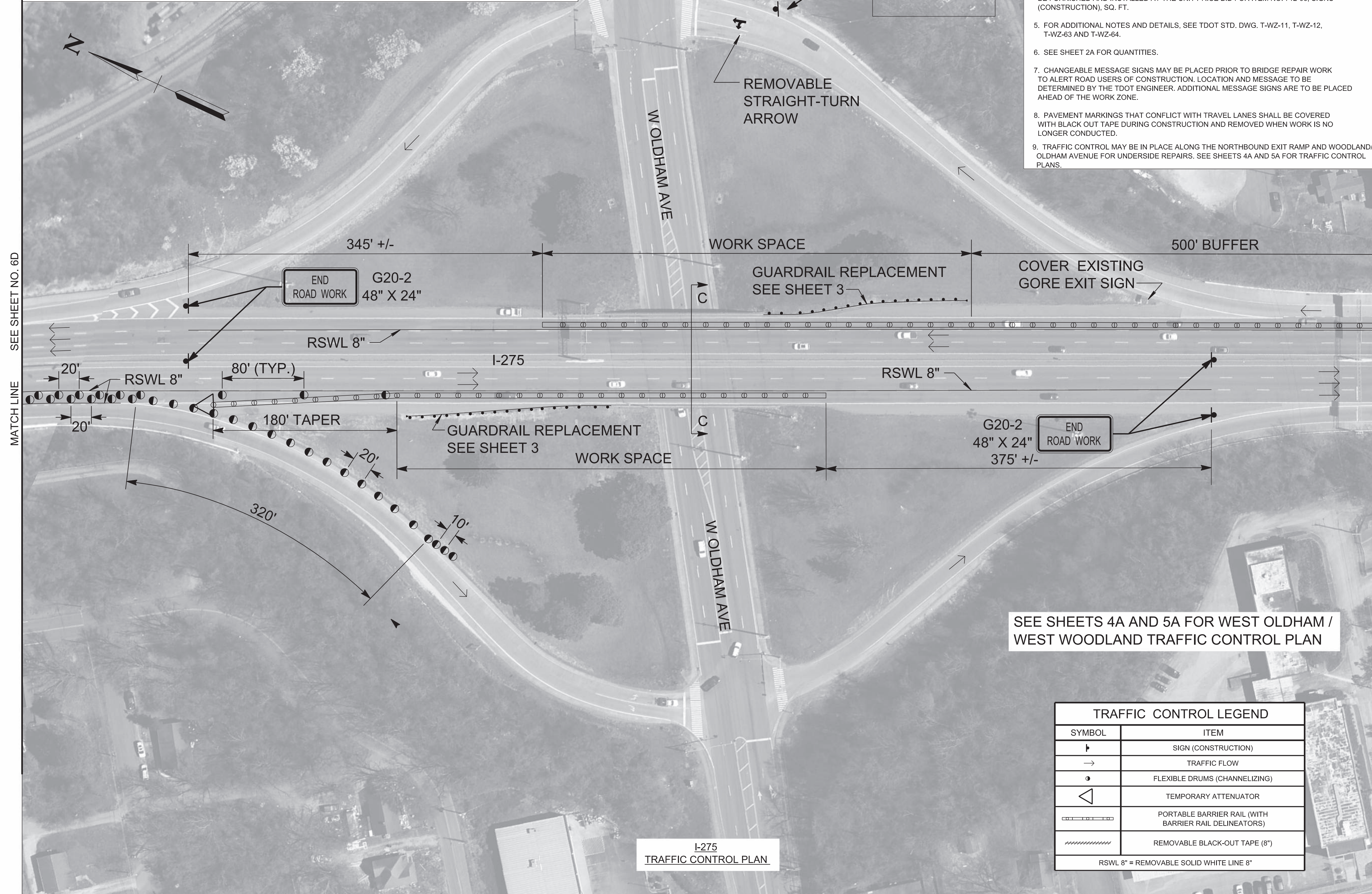
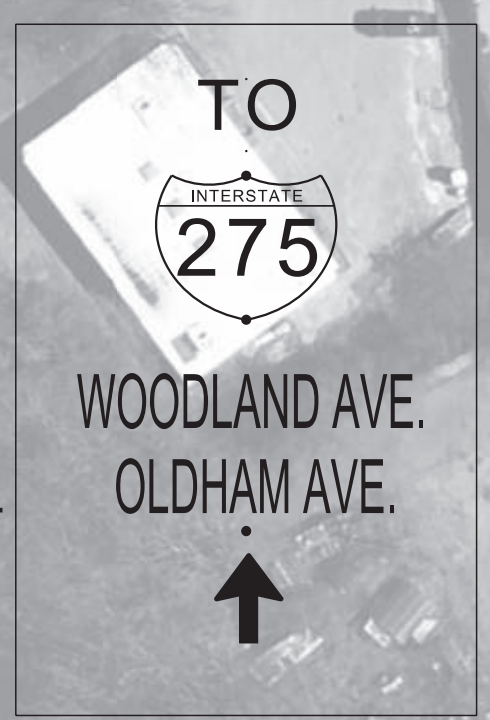
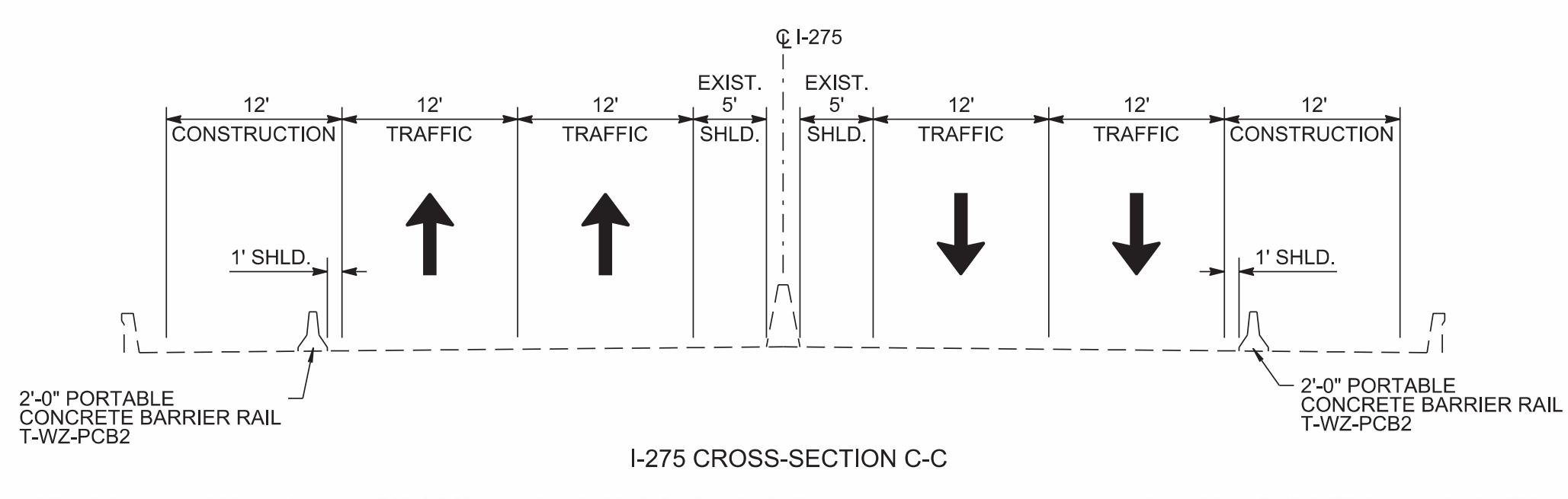
TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
I-275

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	6E
PS&E	2026	471275-M3-004	6E

KNOX COUNTY I-275
 REV. 03-24-2026; ADDED DETOUR SIGN AND PAVEMENT MARKING, ADDED NOTE 9.

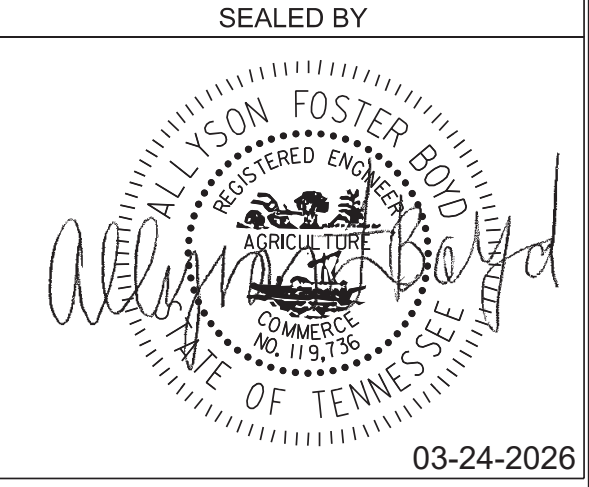
- NOTES:
1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 2. WHEN TRAVEL LANES ON I-275 ARE REDUCED, CONTRACTOR SHALL CONDUCT REPAIRS OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT TO OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
 3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-11, T-WZ-12, T-WZ-63 AND T-WZ-64.
 6. SEE SHEET 2A FOR QUANTITIES.
 7. CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
 8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.
 9. TRAFFIC CONTROL MAY BE IN PLACE ALONG THE NORTHBOUND EXIT RAMP AND WOODLAND/OLDHAM AVENUE FOR UNDERSIDE REPAIRS. SEE SHEETS 4A AND 5A FOR TRAFFIC CONTROL PLANS.



SEE SHEETS 4A AND 5A FOR WEST OLDHAM / WEST WOODLAND TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
▬	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
◁	TEMPORARY ATTENUATOR
▬▬▬▬▬▬	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
////	REMOVABLE BLACK-OUT TAPE (8")

RSWL 8" = REMOVABLE SOLID WHITE LINE 8"

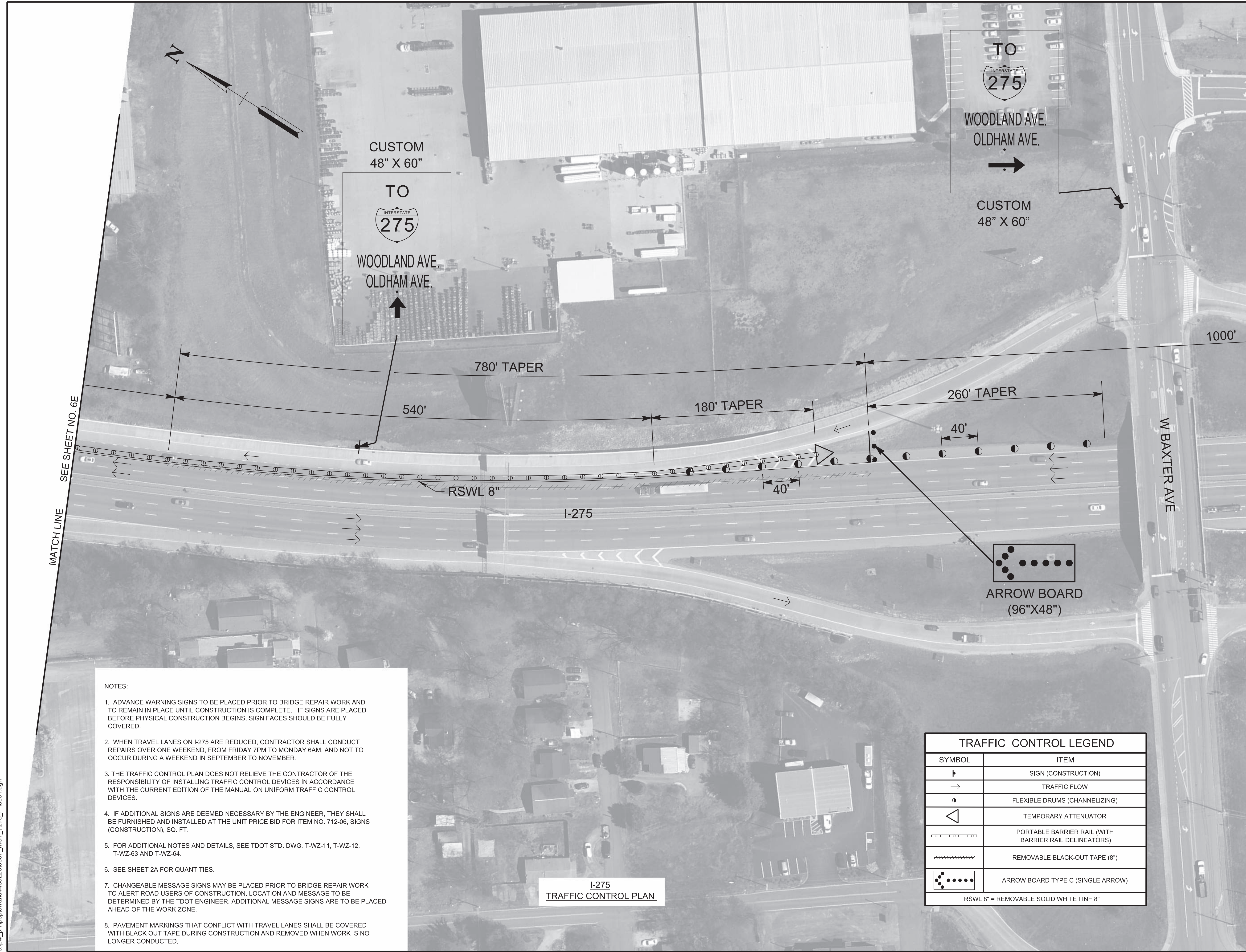


STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-I0275-1.02 L&R
 I-275

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	6F
PS&E	2026	471275-M3-004	6F

KNOX COUNTY I-275



MATCH LINE SEE SHEET NO. 6E

MATCH LINE SEE SHEET NO. 6G

- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 - WHEN TRAVEL LANES ON I-275 ARE REDUCED, CONTRACTOR SHALL CONDUCT REPAIRS OVER ONE WEEKEND, FROM FRIDAY 7PM TO MONDAY 6AM, AND NOT TO OCCUR DURING A WEEKEND IN SEPTEMBER TO NOVEMBER.
 - THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 - FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-11, T-WZ-12, T-WZ-63 AND T-WZ-64.
 - SEE SHEET 2A FOR QUANTITIES.
 - CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
 - PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	FLEXIBLE DRUMS (CHANNELIZING)
	TEMPORARY ATTENUATOR
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	REMOVABLE BLACK-OUT TAPE (8")
	ARROW BOARD TYPE C (SINGLE ARROW)

RSWL 8" = REMOVABLE SOLID WHITE LINE 8"

SEALED BY

02-04-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-I0275-1.02 L&R
I-275

I-275
TRAFFIC CONTROL PLAN

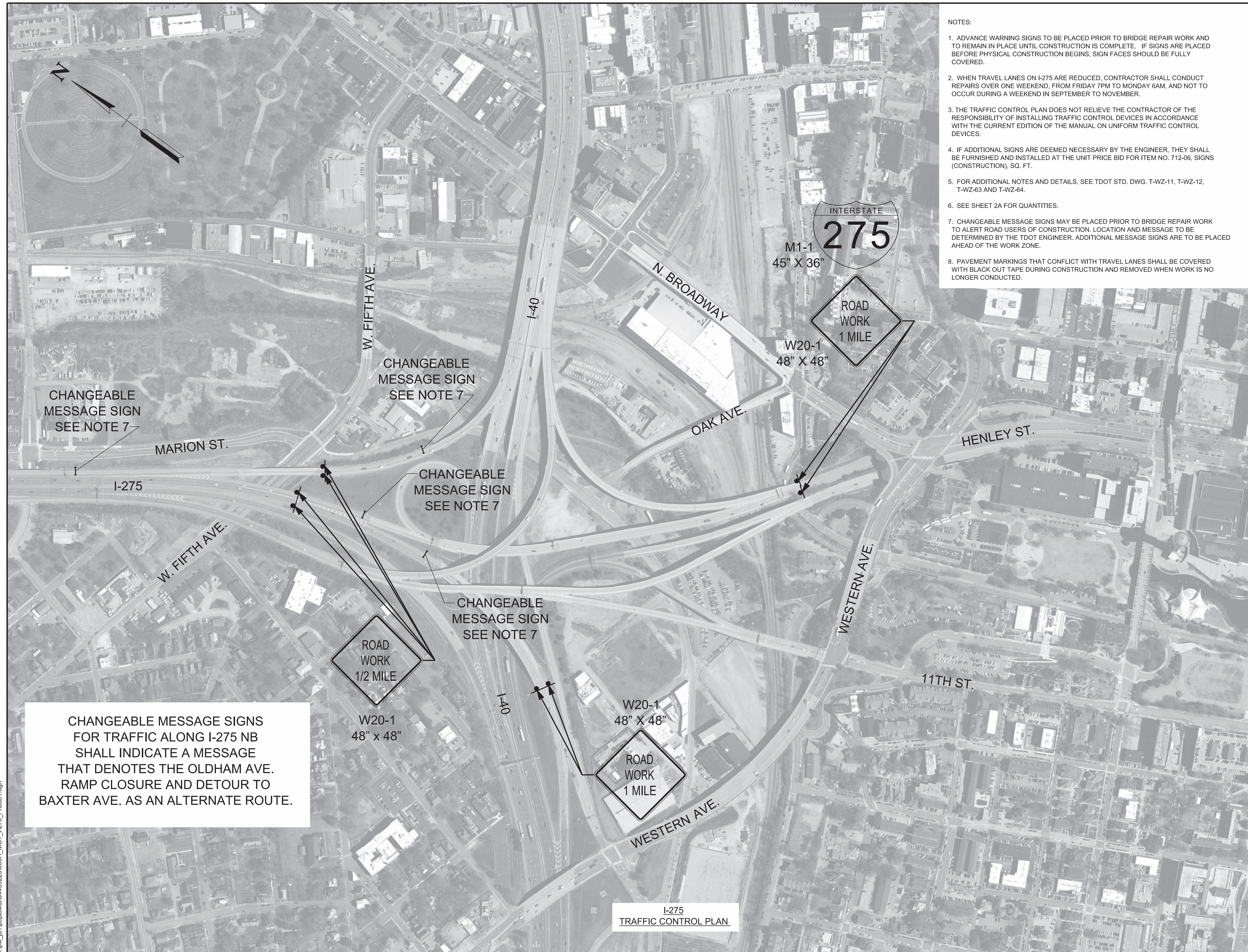
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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	471275-M3-004	6H
PS&E	2026	471275-M3-004	6H

KNOX COUNTY I-275
REV. 03-24-2026: ADDED NOTE FOR CHANGEABLE MESSAGE SIGNS.

NOTES:

1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
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8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.



CHANGEABLE MESSAGE SIGNS FOR TRAFFIC ALONG I-275 NB SHALL INDICATE A MESSAGE THAT DENOTES THE OLDHAM AVE. RAMP CLOSURE AND DETOUR TO BAXTER AVE. AS AN ALTERNATE ROUTE.

I-275
TRAFFIC CONTROL PLAN

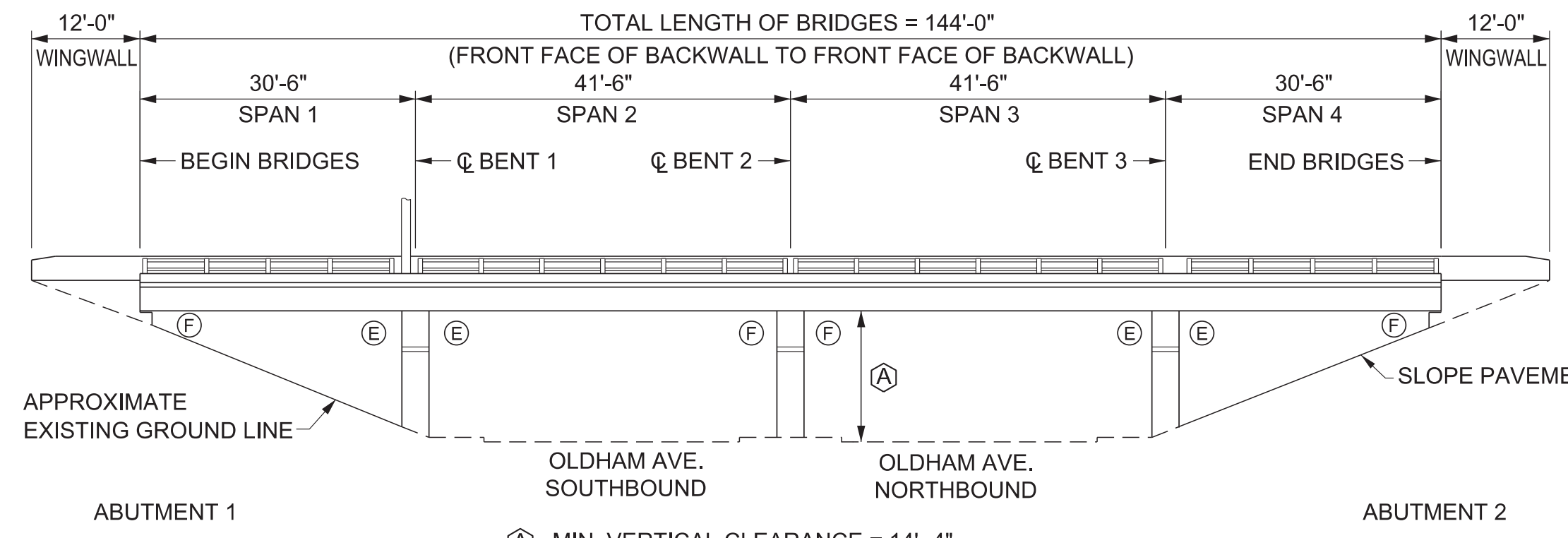
SEALED BY

03-24-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10275-1.02 L&R
I-275

PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	03-24-2026	TAP	ADDED LIST OF REFERENCE DRAWINGS



ⓔ DENOTES EXPANSION
ⓕ DENOTES FIXED

Ⓐ - MIN. VERTICAL CLEARANCE = 14'-4"
(BASED ON SEPTEMBER 2002 TDOT INSPECTION REPORT)

ELEVATION
ALONG ⓕ I-275
1" = 15'-0"

LIST OF BRIDGE DRAWINGS

DRAWING	DWG. NO.	REV. DATE
LAYOUT OF BRIDGES TO BE REPAIRED	BR-133-319	03-24-2026
ESTIMATED BRIDGE QUANTITIES	BR-133-320	03-24-2026
GENERAL NOTES	BR-133-321	03-24-2026
SUPERSTRUCTURE REPAIRS	BR-133-322	
ABUTMENT REPAIRS	BR-133-323	
BENT 1 REPAIRS	BR-133-324	
BENT 2 REPAIRS	BR-133-325	
BENT 3 REPAIRS	BR-133-326	
CONCRETE REPAIR DETAILS	BR-133-327	
BEAM COLLISION REPAIR DETAILS	BR-133-328	
BEAM END REPAIR DETAILS	BR-133-329	

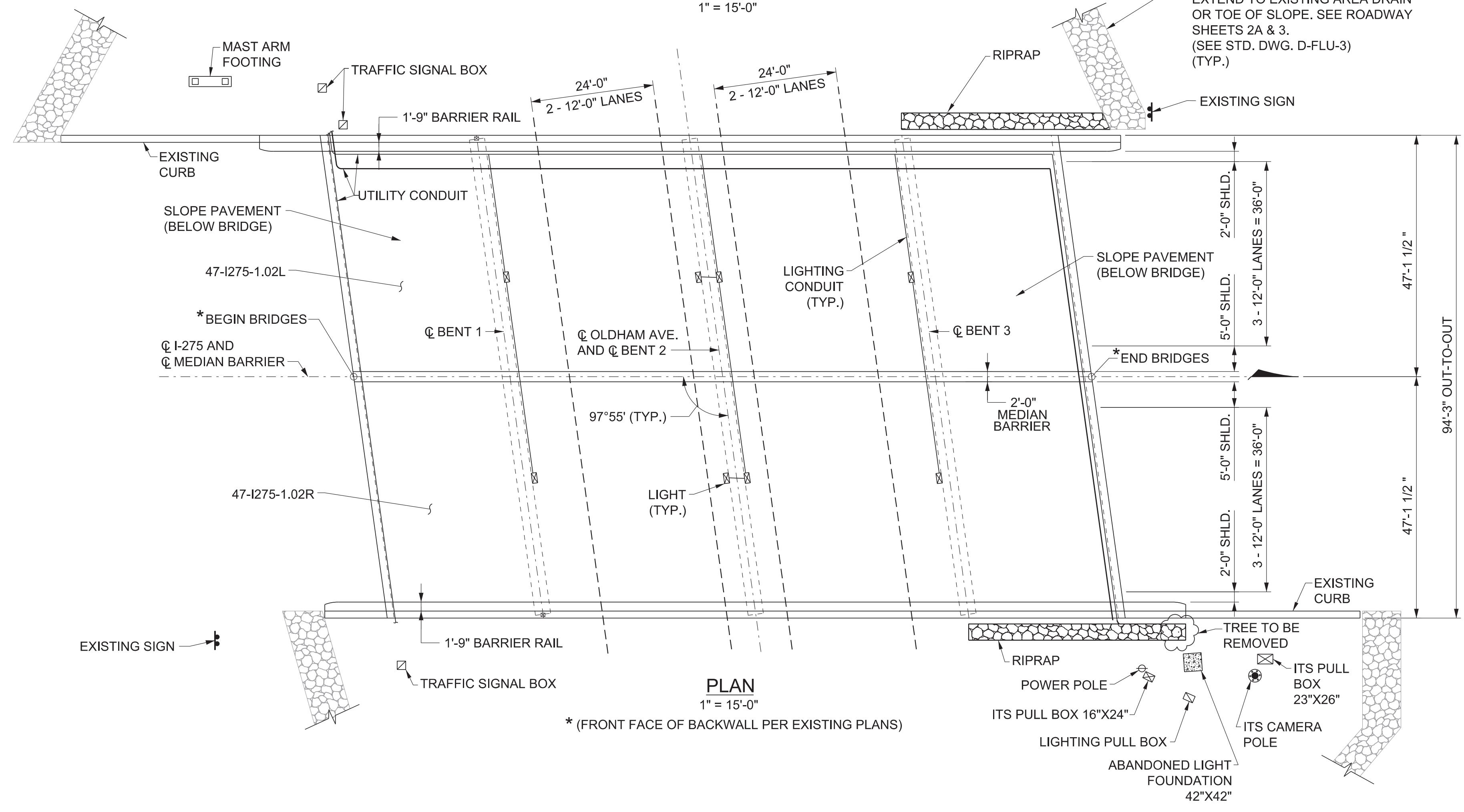
LIST OF REFERENCE DRAWINGS
(TO BE PRINTED WITH PLANS)
PHASE II TMS LAYOUT-SHEET NO. 124

LIST OF BRIDGE REFERENCE DRAWINGS
(TO BE PRINTED WITH PLANS)
F-11-9, F-11-45, F-11-45A THRU F-11-49
BR-27-91, BR-27-92, BR-27-94 THRU BR-27-96

LIST OF SPECIAL PROVISIONS

NO.	REV.	DESCRIPTION
604FRP		FIBER-REINFORCED POLYMER MATERIAL

SEE SHEET 1A FOR STANDARD DRAWINGS



PLAN
1" = 15'-0"

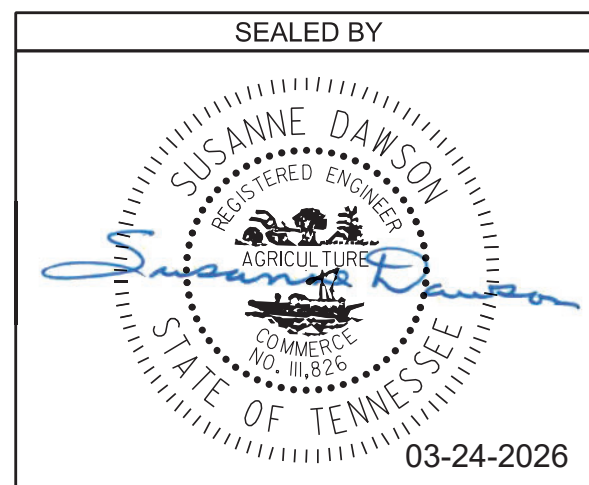
* (FRONT FACE OF BACKWALL PER EXISTING PLANS)

SCOPE OF WORK

- PROVIDE REQUIRED TRAFFIC CONTROL MAINTAINING AT LEAST ONE LANE IN EACH DIRECTION ON OLDHAM AVENUE AT ALL TIMES.
- MAINTAIN EXISTING UTILITIES UNDER BRIDGE AND UNDER BRIDGE LIGHTING.
- REPAIR COLLISION DAMAGE TO EXTERIOR CONCRETE BEAM 6 (RIGHT) IN SPAN 3. USE MECHANICAL BAR SPLICERS FOR DAMAGED REINFORCEMENT (COST TO BE INCLUDED IN ITEM 604-10.80) AND FIBER WRAP DAMAGED AREA.
- REPAIR DETERIORATED AREAS OF CONCRETE ON BEAMS AND SUBSTRUCTURE UNITS.
- EPOXY INJECT CRACKS ON CONCRETE BEAMS, AND SUBSTRUCTURE UNITS.
- CLEAN AND PAINT EXISTING BEARINGS AND EXISTING BEAM END REPAIR PLATES UTILIZING SYSTEM "B".
- HIGH PRESSURE WATER WASH AND TEXTURE COAT ALL EXPOSED SURFACES OF ABUTMENT, WINGWALLS, BENTS INCLUDING TOP SURFACE, BOTTOM AND EXTERIOR BEAM FACES, AND EXTERIOR DECK OVERHANGS.
- REPAIR EROSION GULLY WITH CLASS A-1 RIP RAP AT ABUTMENT 2 RIGHT AND LEFT SIDES ALONG WINGWALL AND EDGE OF SLOPE PAVEMENT. SEE ROADWAY SHEETS 2 AND 3.
- INSTALL RIP RAP FLUME (STD. DWG. D-FLU-3) AT ALL 4 CORNERS OF STRUCTURE AT THE END OF THE WING WALLS ON ABUTMENT 1 RIGHT AND ABUTMENT 2 LEFT, AND AT THE END OF THE EXISTING CURB AT ABUTMENT 1 LEFT AND ABUTMENT 2 RIGHT. SEE ROADWAY SHEETS 2 & 3.
- REMOVE ALL DIRT AND CONCRETE DEBRIS THAT HAS FALLEN FROM BRIDGE ALONG OLDHAM AVENUE (COST TO BE INCLUDED IN ITEM 604-10.54).
- REMOVE PLYWOOD FORMS FROM PREVIOUS DECK REPAIRS (COST TO BE INCLUDED IN ITEM 604-10.54).
- REMOVE VEGETATION AND TREE ADJACENT TO ABUTMENT 2 RIGHT SIDE (COST TO BE INCLUDED IN ITEM 201-07.01).
- PERFORM CONCRETE BEAM END REPAIRS (COST TO BE INCLUDED IN ITEM NO. 602-10.32), ENDS OF BEAMS TO BE SUPPORTED DURING REPAIRS (COST TO BE INCLUDED IN ITEM NO. 604-10.26).
- GROUT EXISTING HOLES IN WINGPOSTS AT ABUTMENT 2 LEFT SIDE AND ABUTMENT 1 RIGHT SIDE. DRILL HOLES FOR NEW GUARDRAIL ATTACHMENT UTILIZING PLATE DETAILS ON STD. DWG. S-AGT-1 THRU 7 (COST TO BE INCLUDED IN ROADWAY ITEM 705-06.28).
- REPAIR WASHOUT AREA UNDER SOUTHEAST SIDE OF EXISTING SLOPE PAVING (COST TO BE INCLUDED IN ITEM 604-10.55).

TRAFFIC DATA	
I-275	
POSTED SPEED LIMIT	55 M.P.H
AADT (2024) NB	35,961
AADT (2024) SB	37,946

TRAFFIC DATA	
OLDHAM AVENUE	
POSTED SPEED LIMIT	30 M.P.H
AADT (2024) NB	5,806
AADT (2024) SB	6,235



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT OF BRIDGES
TO BE REPAIRED
BRIDGE NO. 47-I0275-1.02 L & R
FEDERAL BRIDGE ID NOS.
47102750007 AND 47102750008
I-275 OVER OLDHAM AVENUE

KNOX COUNTY
2026

BR-133-319

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PIN NO.:	083980.01	DATE:	AUGUST 2025
DESIGN BY:	BRAD WARREN	DATE:	AUGUST 2025
DRAWN BY:	T. PELOW/D. PICKEL	DATE:	AUGUST 2025
SUPERVISED BY:	SUSANNE DAWSON	DATE:	AUGUST 2025
CHECKED BY:	FRANK BALE	DATE:	AUGUST 2025



ESTIMATED BRIDGE QUANTITIES

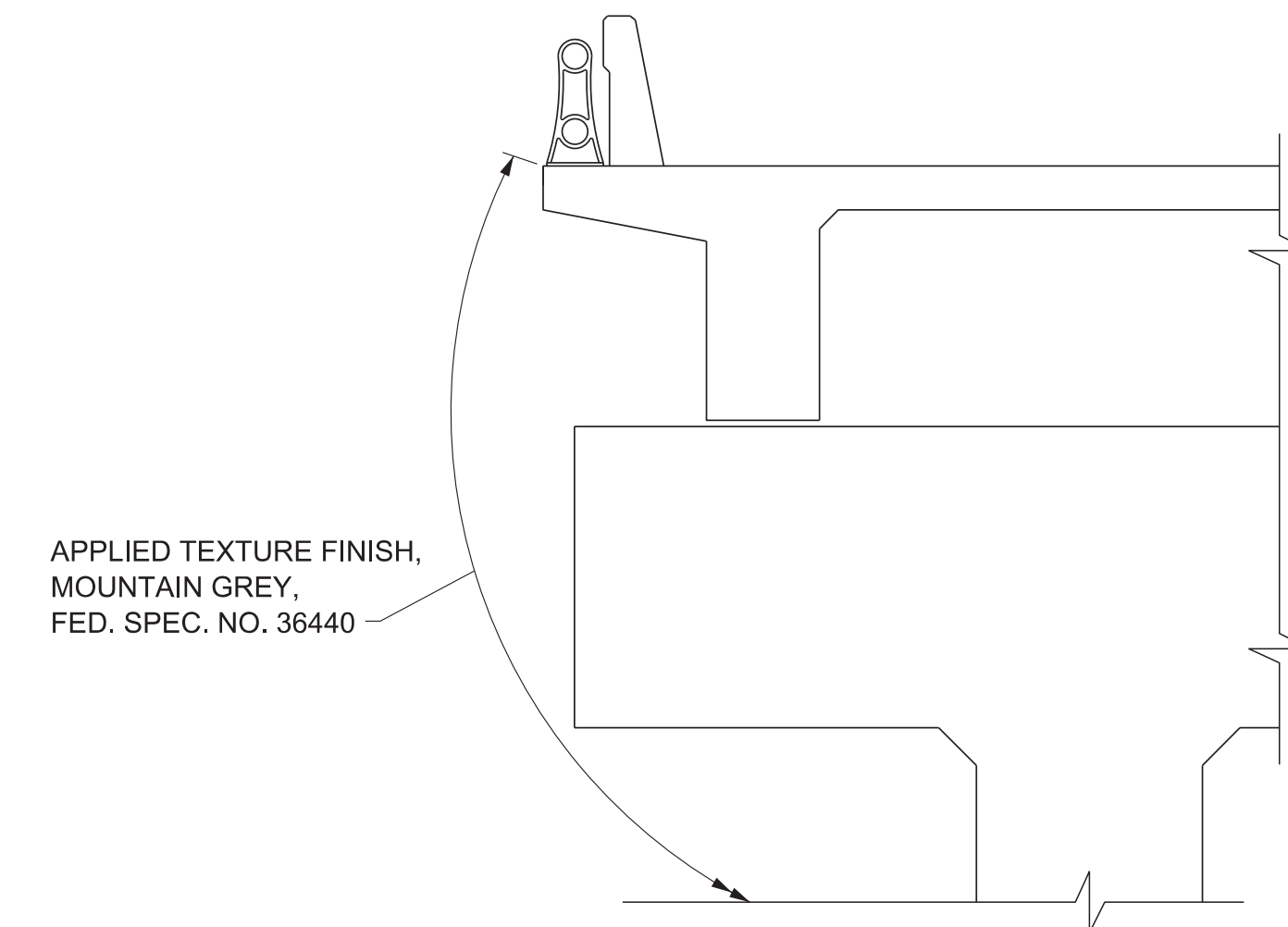
ITEM NO.	DESCRIPTION	UNIT	47-1275-1.02L	47-1275-1.02R	TOTAL
1	201-07.01 REMOVAL AND DISPOSAL OF BRUSH & TREES	L.S.	0	1	1
2	602-10.32 STRUCTURAL STEEL (REPAIRS)	L.B.	443	188	631
3	603-02.15 REPAINT EXISTING BEARINGS	L.S.	0.5	0.5	1
4	603-02.20 SPOT PAINTING EXISTING STEEL STRUCTURES	S.F.	74	74	148
5	603-05.20 CONTAINMENT AND DISPOSAL OF WASTE (BR. NO.47-10275-1.02 L & R)	L.S.	0.5	0.5	1
6	604-04.02 APPLIED TEXTURE FINISH (EXISTING STRUCTURES)	S.Y.	485	485	970
7 *	604-10.05 CONCRETE	S.F.	197	162	359
8	604-10.26 BRACING	EACH	5	2	7
7 *	604-10.42 CONCRETE REPAIRS	C.F.	43	22	65
7, 13 *	604-10.54 CONCRETE REPAIRS	S.F.	197	162	359
*	604-10.55 CONCRETE (FOUNDATION REPAIRS)	C.Y.	8	0	8
	604-10.58 EPOXY INJECTION (INJECTION)	GAL.	4	5	9
*	604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)	L.F.	72	98	170
7, 9, 10, 12	604-10.80 BRIDGE REPAIRS	L.S.	0	1	1
9, 11	604-10.83 COMPOSITE FIBER ENCASEMENT	S.F.	0	203	203

* DENOTES ITEM CAN BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

FOOTNOTES:

- INCLUDES THE COST OF ALL LABOR AND MATERIALS NECESSARY FOR THE REMOVAL AND DISPOSAL OF TREE AND NEARBY VEGETATION AS SHOWN IN LAYOUT ON SHEET BR-133-319. WHERE POSSIBLE, STUMPS AND ROOTS ARE TO REMAIN TO PREVENT GROUND DISTURBANCE. ANY DAMAGE TO VEGETATED AREAS NOT SPECIFICALLY MENTIONED WITHIN THE PROJECT SCOPE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THESE AREAS ARE TO BE RETURNED TO THEIR PRE-CONSTRUCTION STATE AND SHALL BE DETERMINED BY THE ENGINEER. ALL COSTS (LABOR AND MATERIAL) ASSOCIATED WITH THIS WORK SHALL BE INCLUDED IN ITEMS BID ON.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR THE ASSEMBLY, INSTALLATION, AND GALVANIZING OF NEW CONCRETE BEAM END REPAIRS ON BRIDGES 47-1275-1.02L AND 47-1275-1.02R.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR THE SURFACE PREPARATION AND PAINTING OF STEEL BEARING DEVICES AT ALL PIERS AND ABUTMENTS. SEE NOTES ON BR-133-321.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR SURFACE PREPARATION AND SPOT PAINTING OF EXISTING STRUCTURAL STEEL AT BEAM END REPAIR AREAS AS INDICATED IN SCOPE OF WORK. SEE SURFACE PREPARATION AND PAINTING NOTES ON BR-133-321.
- INCLUDES CONTAINMENT AND DISPOSAL OF ALL WASTE GENERATED DURING CLEANING AND PAINTING OF STEEL BEARINGS AT PIERS, ABUTMENTS, AND EXISTING STRUCTURAL STEEL AT BEAM END REPAIR AREAS. SEE NOTES ON BR-133-321.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR THE APPLICATION OF ALL TEXTURE COATING FOR THE FULL LENGTH OF THE BRIDGE AS SHOWN IN THE TEXTURE COATING DETAIL ON THIS SHEET. ALSO INCLUDES SURFACE PREPARATION USING A HIGH PRESSURE WASH TO REMOVE ALL LOOSE COATINGS, FLAKING AND OTHER FOREIGN SUBSTANCES TO THE FULL SATISFACTION OF THE ENGINEER.
- EXISTING LIGHTING AND ALL CONDUIT IN CONFLICT WITH REPAIRS TO BE TEMPORARILY RELOCATED AS NECESSARY TO FACILITATE PLACEMENT OF CONCRETE REPAIR. ALL LABOR AND MATERIALS NECESSARY TO RELOCATE CONDUIT SHALL BE PAID FOR UNDER ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.
- INCLUDES ALL COSTS FOR SUPPORTING BEAM ENDS DURING REPAIRS. SEE NOTE ON BR-133-329.
- ITEMS REQUIRED FOR COLLISION DAMAGE REPAIR.
- ITEM INCLUDES COST OF ALL LABOR, MATERIALS, AND INCIDENTALS FOR THE BEAM REPAIR AS SHOWN ON SHEET BR-133-328, EXCEPT FOR FIBER WRAP. FIBER WRAP TO BE PAID FOR UNDER ITEM NO. 604-10.83, COMPOSITE FIBER ENCASEMENT.
- INCLUDES COST OF ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS TO PLACE FIBER REINFORCED POLYMER WRAP AS SHOWN ON SHEET BR-133-328 TO BEAM 6R. INCLUDES THE FURNISHING OF ALL SUBMITTALS, MATERIALS, TOOLS, EQUIPMENT, LABOR, SURFACE PREPARATION, TRANSPORTATION, STORAGE, AND SUPERVISION REQUIRED FOR THE APPLICATION OF THE FIBER REINFORCED POLYMER WRAP MATERIAL AS INDICATED IN TDOT SPECIAL PROVISION 604FRP. INSTALLATION SHALL FOLLOW MANUFACTURER'S RECOMMENDATIONS AND MEET WITH THE FULL SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING BEAM REPAIRS. DETAILS OF ANY TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE THE BEGINNING OF ANY BEAM REPAIR.
- INCLUDES COST OF REMOVING DIRT AND FALLEN CONCRETE DEBRIS ALONG OLDHAM AVENUE, AS WELL AS REMOVING PLYWOOD FORMS FROM PREVIOUS DECK REPAIRS.

PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	03-24-2026	DPP	UPDATED ITEM ORDER



APPLIED TEXTURE FINISH SKETCH

LEFT SIDE ONLY SHOWN, SKETCH IS SYMMETRIC ABOUT @ BRIDGE.

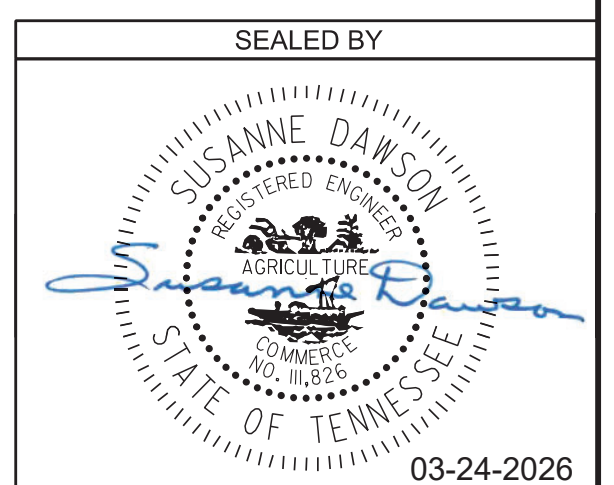
HIGH PRESSURE WATER WASH AND TEXTURE COAT ALL EXPOSED EXISTING CONCRETE SURFACES OF ABUTMENT, WINGWALLS, BENTS INCLUDING TOP AND BOTTOM SURFACES, BOTTOM AND EXTERIOR BEAM FACES, AND EXTERIOR DECK OVERHANGS. ALL SURFACES SHALL BE MOUNTAIN GREY (AMS-STD-595A, COLOR NO. 36440).

COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NO. 604-04.02, APPLIED TEXTURE FINISH (EXISTING STRUCTURES), S.Y.

THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.

THE EXISTING SURFACES THAT ARE TO RECEIVE A TEXTURE FINISH SHALL BE FREE OF ALL EFFLORESCENCE, FLAKING TEXTURE COATING, RUST, DIRT, OIL, AND OTHER FOREIGN SUBSTANCES PRIOR TO THE APPLICATION OF THE TEXTURE FINISH. THE SURFACE SHALL BE CLEANED TO THE COMPLETE SATISFACTION OF THE ENGINEER USING A HIGH PRESSURE WATER WASH. COST TO BE INCLUDED IN ITEM NO. 604-04.02, APPLIED TEXTURE FINISH (EXISTING STRUCTURES), S.Y.

THE WASH WATER IS TO BE FILTERED AND PAINT CHIPS AND DEBRIS COLLECTED PRIOR TO RELEASE OF WATER.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED BRIDGE QUANTITIES

BRIDGE NO. 47-10275-1.02 L & R
FEDERAL BRIDGE ID NOS.
47102750007 AND 47102750008
I-275 OVER OLDHAM AVENUE

KNOX COUNTY
2026

BR-133-320

PIN NO.: 083980.01
 DESIGN BY: BRAD WARREN DATE: AUGUST 2025
 DRAWN BY: T. PELOW/D. PICKEL DATE: AUGUST 2025
 SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2025
 CHECKED BY: FRANK BALE DATE: AUGUST 2025

GENERAL NOTES

SPECIFICATIONS & LOADING

CONSTRUCTION SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (APRIL 1, 2026 EDITION), AND THE 4TH EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.

DESIGN SPECIFICATIONS: 10TH EDITION (2024) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2ND EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

LOADING:

- A. HL-93 LIVE LOAD
- B. DEAD LOAD INCLUDES 35 LB/SQ. FT. FOR FUTURE WEARING SURFACE

STEEL, CONCRETE, REINFORCING, AND FORMING

STRUCTURAL STEEL: : SHALL CONFORM TO ASTM A709 GRADE 50 UNLESS OTHERWISE NOTED.

REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.

NOTE: MECHANICAL BAR SPLICERS MUST BE ON THE TDOT QUALIFIED PRODUCTS LIST 27. THE BAR SPLICERS SHALL MEET AASHTO LRFD SPECIFICATIONS FOR MECHANICAL CONNECTION. WHEN EPOXY COATING IS REQUIRED, THE EXPOSED THREADS SHALL BE REPAIRED AFTER SPLICING ACCORDING TO SECTION 907 OF THE STANDARD SPECIFICATIONS. THE COST OF FURNISHING THE BAR SPLICERS, (AND EPOXY COATING WHEN REQUIRED) INCLUDING ALL LABOR AND MATERIALS NECESSARY FOR COMPLETE INSTALLATION, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM NO. 604-10.80, BRIDGE REPAIRS, L.S.

CONCRETE: TO BE CLASS A F'C = 3000 PSI EXCEPT AS NOTED OTHERWISE.

HIGH EARLY STRENGTH CONCRETE: THE MIX IS TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, CLASS X. THE CEMENT CONTENT SHALL BE A MINIMUM OF 714 LBS. THE WATER-CEMENT RATIO SHALL BE A MAXIMUM OF 0.40. DESIGN AIR CONTENT SHALL BE 6% WITH ±2% ACCEPTANCE RANGE IN THE FIELD. SLUMP SHALL BE 3±1 INCHES. IF USING A TYPE A, F, OR G WATER REDUCER, THE SLUMP SHALL BE MAXIMUM OF 8 INCHES. NO FLY ASH REPLACEMENT WILL BE PERMITTED. THE MINIMUM 28 DAY COMPRESSIVE STRENGTH SHALL BE 3,500 PSI. TRAFFIC SHALL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST SPECIMENS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF TEN (10) DAYS.

CONCRETE CURING: ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

QUICK-SET PATCHING MATERIAL: QUICK-SET PATCHING MATERIAL SHALL BE A POLYMER MODIFIED CEMENTITIOUS PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13.009 POLY MOD CEMENT STRUCT PATCH VERT & OVER FOR ACCEPTABLE PATCHING MATERIALS.

MISCELLANEOUS GENERAL NOTES

SHOP DRAWINGS: REFER TO SECTION 105.02 OF THE STANDARD SPECIFICATIONS. IF USING PAPER COPIES, SHOP DRAWINGS ARE TO BE SENT TO THE BRIDGE REPAIR OFFICE IN THE DIVISION OF STRUCTURES, FOR ELECTRONIC SUBMITTALS, SEE SECTION 105.02 OF THE STANDARD SPECIFICATIONS. EACH SHOP DRAWING SHALL CONTAIN IN THE TITLE BLOCK THE FOLLOWING: THE STATE PROJECT NUMBER, COUNTY, BRIDGE NAME, BRIDGE NUMBER (OR STRUCTURE TYPE AND NUMBER), STATION, AND CONTRACT NUMBER. SHOP DRAWINGS WITH TITLE BLOCKS NOT INCLUDING THE FOREGOING IDENTIFICATION WILL BE RETURNED FOR CORRECTION BEFORE ANY REVIEWS FOR APPROVAL ARE CONDUCTED.

DEMOLITION: THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE AS LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED.

THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM), MINI EXCAVATOR, OR OTHER EQUIPMENT FOR ANY CONCRETE REMOVAL.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

ANY AREA THAT IS DISTURBED OUTSIDE THE LIMITS OF THE CONSTRUCTION DURING THE LIFE OF THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

CONTAINMENT AND DISPOSAL: OUR RECORDS SHOW THAT THIS BRIDGE HAS OR HAS HAD LEAD-BASED/CHROMATE-BASED PAINT APPLIED TO IT DURING ITS HISTORY; THEREFORE, THE CONTRACTOR SHALL ASSUME THAT REMNANTS OF THAT PAINT REMAIN ON THE BRIDGE. SEE SECTION 603.13 OF THE STANDARD SPECIFICATIONS FOR SPECIAL PRECAUTIONS THAT MUST BE TAKEN IN THE REMOVAL; CONTAINMENT AND DISPOSAL OF THE SURFACE PREPARATION WASTE AND PAINT REMOVAL MEDIA; AND WORKER AND PUBLIC SAFETY.

SPECIAL NOTE SURFACE PREPARATION FOR PAINT: OUR RECORDS SHOW THAT THIS BRIDGE HAS OR HAS HAD LEAD-BASED/CHROMATE-BASED PAINT APPLIED TO IT DURING ITS HISTORY. THEREFORE, THE CONTRACTOR SHALL ASSUME THAT REMNANTS OF THAT PAINT REMAIN ON THE BRIDGE ALONG WITH THE POSSIBILITY OF THE PRESENCE OF MILLSCALE. CONTRACTOR SHALL BID ACCORDINGLY.

WORKER PROTECTION: OUR MAINTENANCE RECORDS INDICATE THAT THIS BRIDGE WAS ORIGINALLY PAINTED WITH MATERIALS CONTAINING LEAD AND/OR CHROMATES AND THE CONTRACTOR SHALL BE REQUIRED TO PROCEED ACCORDINGLY AND TAKE ALL MANDATORY SAFEGUARDS PRESCRIBED BY THE STATE AND FEDERAL LAW FOR WORKER PROTECTION AND HAZARDOUS MATERIALS DISPOSAL.

PAINT: SHALL BE SYSTEM "B" (QPL 3.005) IN ACCORDANCE WITH SECTION 603.06 OF THE STANDARD SPECIFICATIONS. COLOR OF THE FINISH COAT SHALL COMPLY WITH AMS-STD-595A, FEDERAL STANDARD NO. 36440, GRAY. SEE SECTIONS 603 AND 910 OF THE STANDARD SPECIFICATIONS. ALL PRODUCTS USED IN THIS COATING SYSTEM, INCLUDING THINNERS, MUST BE SUPPLIED BY THE SAME MANUFACTURER.

BEARING CLEANING: ALL STEEL BEARINGS SHALL BE CLEANED AND PAINTED. CLEANING SHALL BE IN ACCORDANCE WITH SECTION 603.05(B.1) OF THE STANDARD SPECIFICATIONS. HAND (SSPC-SP 2) OR POWER (SSPC-SP 3) TOOL CLEANING SHALL REMOVE ALL RUST, SCALE, LOOSE PAINT, AND DIRT. THE COLOR OF THE FINISH COAT SHALL COMPLY WITH AMS-STD-595A, FEDERAL STANDARD NO. 36440, GRAY. SEE SECTIONS 603 AND 910 OF THE STANDARD SPECIFICATIONS.

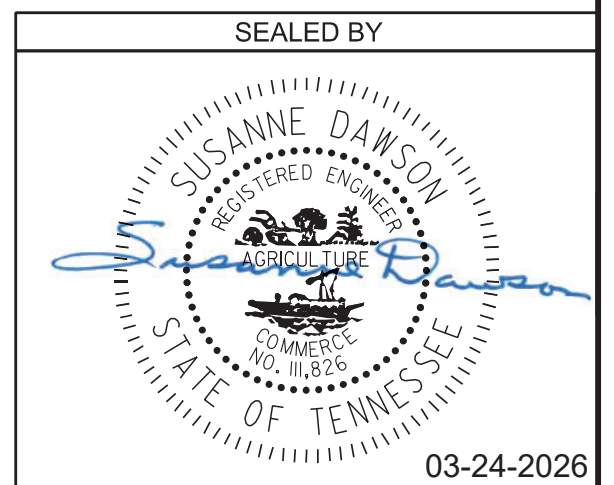
APPLICATION: THE COATING APPLICATOR SHALL FOLLOW THE MANUFACTURER'S PRINTED INSTRUCTIONS AND SHALL HAVE THESE INSTRUCTIONS ON SITE DURING THE COURSE OF THE WORK.

FORMS AND FALSEWORK: CONCRETE FORM WORK, FALSEWORK, AND TEMPORARY SUPPORTS SHALL BE REMOVED FROM THE JOB SITE AFTER WORK IS COMPLETED. COST OF FORMS, FALSEWORK, AND TEMPORARY SUPPORTS SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.

SPOT CLEANING AND PAINTING: AREAS REQUIRING REPAINTING OF STEEL SHALL BE CLEANED IN ACCORDANCE WITH SECTION 603.05(B.1) OF THE STANDARD SPECIFICATIONS. HAND (SSPC-SP 2) OR POWER (SSPC-SP 3) TOOL CLEANING SHALL REMOVE ALL RUST, SCALE, LOOSE PAINT, AND DIRT. THESE AREAS SHALL BE SPOT PRIMED IMMEDIATELY AFTER CLEANING. CLEANED AND PRIMED STEEL SHALL RECEIVE A SYSTEM "B" (QPL 3.005) PAINT SYSTEM IN ACCORDANCE WITH SECTION 603.06(B) OF STANDARD SPECIFICATIONS. THE COLOR OF THE FINISH COAT SHALL COMPLY WITH AMS-STD-595A, FEDERAL STANDARD NO. 36440, GRAY. SEE SECTIONS 603 AND 910 OF THE STANDARD SPECIFICATIONS.

SEE THE TDOT QUALIFIED PRODUCTS LIST 3.005 FOR ACCEPTABLE COATINGS FOR THE PAINT SYSTEM. THE SAME MANUFACTURER SHALL SUPPLY ALL PRODUCTS USED, INCLUDING THINNERS.

PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	03-24-2026	TAP	UPDATED STANDARD SPECIFICATIONS



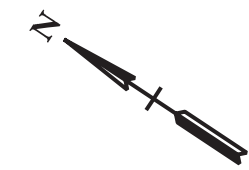
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
GENERAL NOTES

BRIDGE NO. 47-10275-1.02 L & R
FEDERAL BRIDGE ID NOS.
47102750007 AND 47102750008
I-275 OVER OLDHAM AVENUE

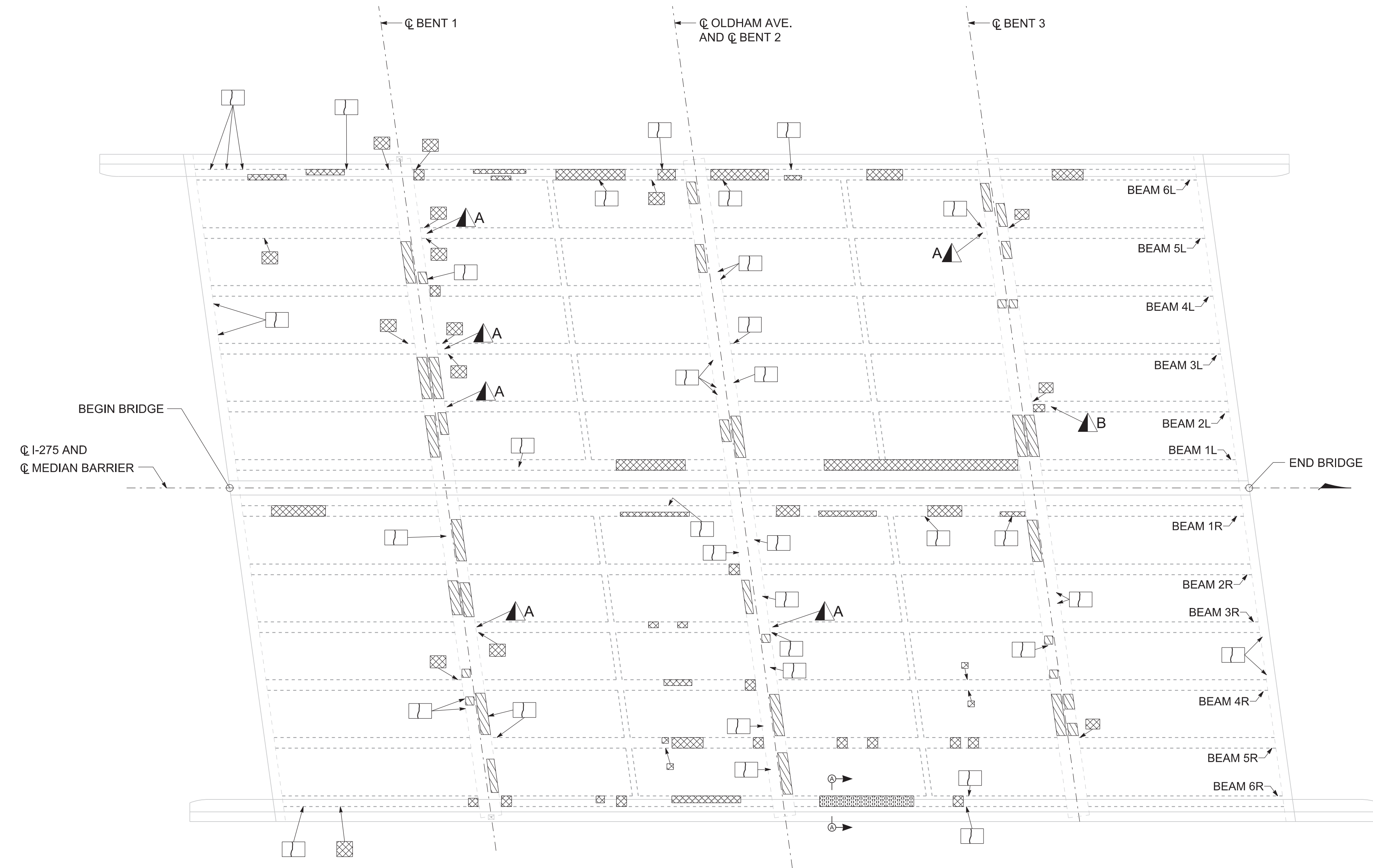
KNOX COUNTY
2026

BR-133-321

PIN NO.: 083980.01
DESIGN BY: BRAD WARREN DATE: AUGUST 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: AUGUST 2025
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2025
CHECKED BY: FRANK BALE DATE: AUGUST 2025

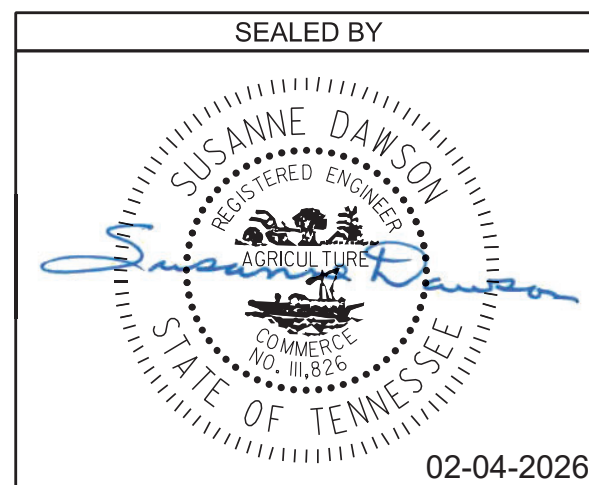


PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



- DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-133-327.
 - DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.42. REPAIR DOES NOT INCLUDE FIBER WRAP. SEE REPAIR DETAILS ON DWG. NO. BR-133-328.
 - DENOTES CRACK TO BE EPOXY INJECTED UNDER ITEM NO. 604-10.62. SEE REPAIR DETAILS ON DWG. NO. BR-133-327.
 - DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.80 AS PART OF COLLISION DAMAGE REPAIR. SEE SECTION A-A AND REPAIR DETAILS ON DWG. NO. BR-133-328.
 - DENOTES LOCATION AND TYPE OF CONCRETE BEAM END REPAIR. SEE REPAIR DETAILS ON DWG. NO. BR-133-329.
- ALL DAMAGES SHOWN ABOVE BENTS INDICATE DAMAGES TO SUPERSTRUCTURE DIAPHRAGM.

PLAN
(SCALE: 1" = 10")
ALL DAMAGES SHOWN ARE BELOW DECK UNLESS OTHERWISE NOTED



ESTIMATED QUANTITIES					
LOCATION OF REPAIR	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE) APPROX. REPAIR AREAS (L.F.)	ITEM NO. 604-10.05/604-10.54 CONCRETE REPAIRS APPROX. REPAIR AREAS (S.F.)	ITEM NO. 604-10.42 CONCRETE REPAIRS APPROX. REPAIR AREAS (C.F.)	ITEM NO. 604-10.80 BRIDGE REPAIRS (L.S.)	ITEM NO. 602-10.32 STRUCTURAL STEEL (REPAIRS) APPROX. REPAIR AREAS (LB.)
LEFT BRIDGE	39	97	43	-	443
RIGHT BRIDGE	69	87	22	1	188

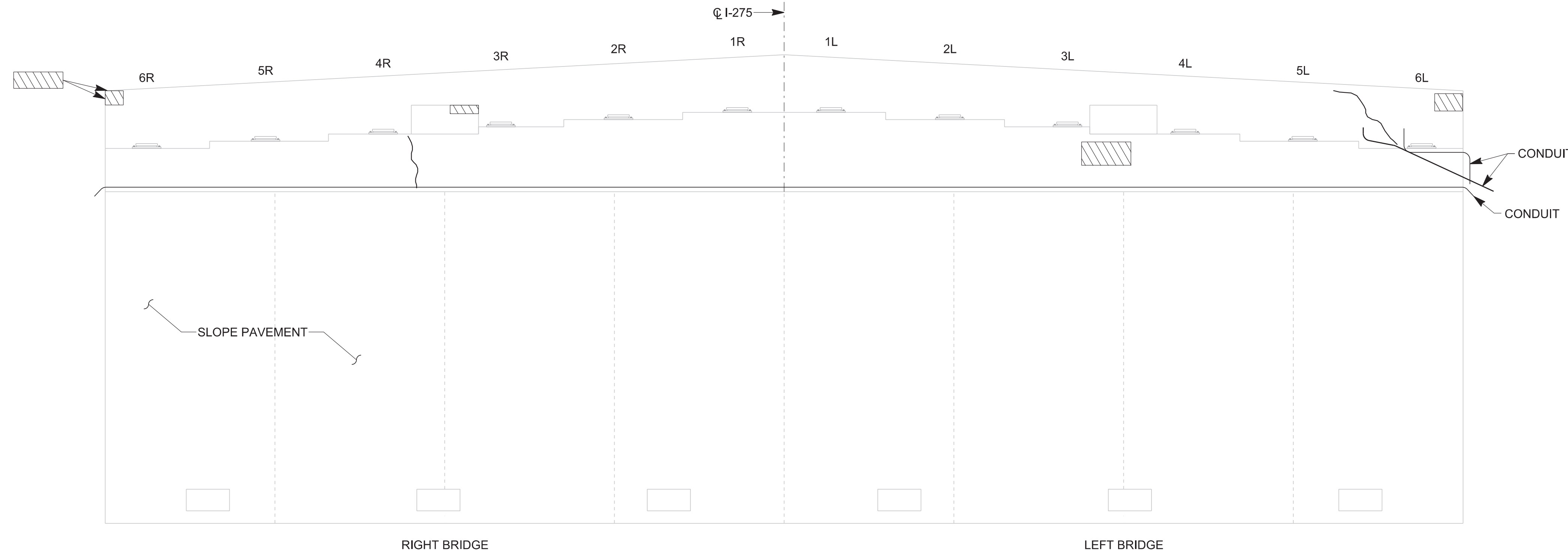
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE REPAIRS
BRIDGE NO. 47-10275-1.02 L & R
FEDERAL BRIDGE ID NOS.
47102750007 AND 47102750008
I-275 OVER OLDHAM AVENUE
KNOX COUNTY
2026

BR-133-322

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PIN NO.: 083980.01
DESIGN BY: BRAD WARREN DATE: AUGUST 2025
DRAWN BY: T. PELOWID, PICKEL DATE: AUGUST 2025
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2025
CHECKED BY: FRANK BALE DATE: AUGUST 2025

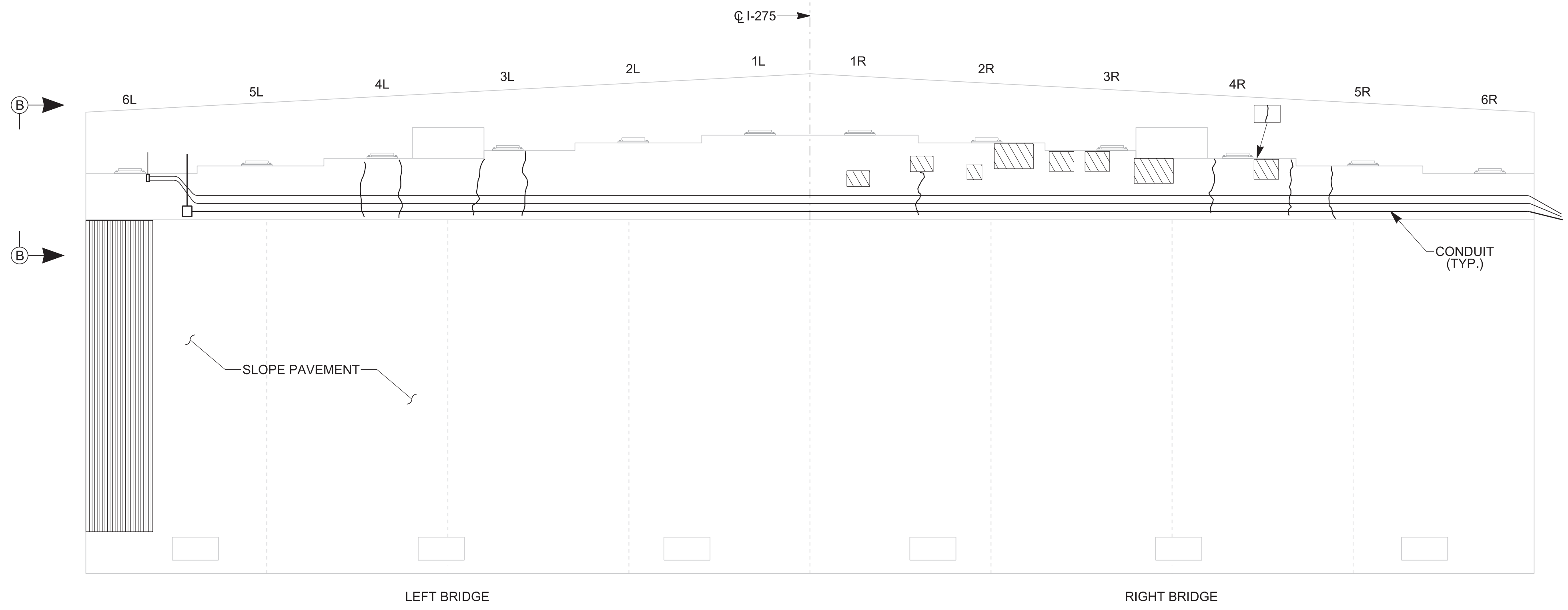
PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



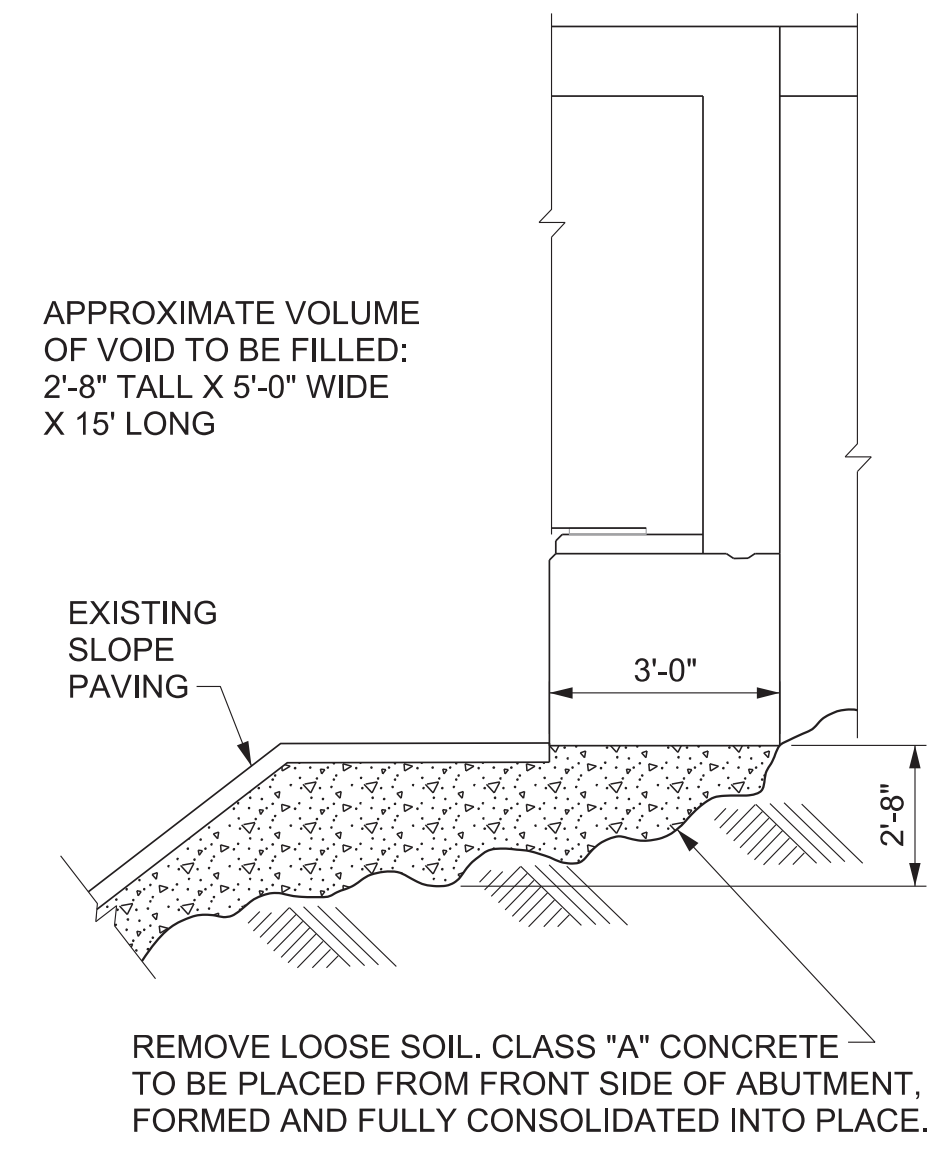
ABUTMENT 1
(SCALE: 1" = 5')
LOOKING BACK

- DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-133-327.
- DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-133-327.
- DENOTES LOCATION TO BE REPAIRED BY PLACEMENT OF CONCRETE UNDER ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS), C.Y. SEE FILL SETTLEMENT DETAIL (THIS SHEET).

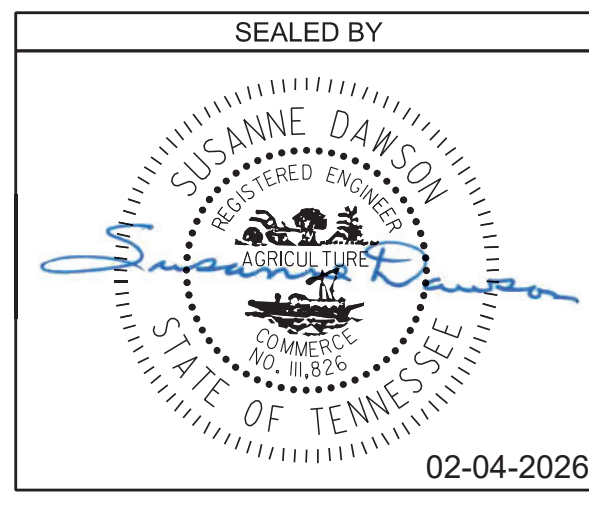
ESTIMATED QUANTITIES			
LOCATION OF REPAIR	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE) APPROX. REPAIR AREAS (L.F.)	ITEM NO. 604-10.05/604-10.54 CONCRETE REPAIRS APPROX. REPAIR AREAS (S.F.)	ITEM NO. 604-10.55 CONCRETE (FOUNDATION REPAIRS) APPROX. REPAIR AREAS (C.Y.)
ABUT. 1 LEFT	2	4	-
ABUT. 1 RIGHT	3	7	-
ABUT 2. LEFT	4	-	8
ABUT 2. RIGHT	15	10	-



ABUTMENT 2
(SCALE: 1" = 5')
LOOKING AHEAD



SECTION B-B
FILL WASHOUT REPAIR DETAIL
ALL COST ASSOCIATED WITH REPAIR OF WASHOUT AREA, INCLUDING ALL LABOR AND MISCELLANEOUS MATERIALS, SHALL BE INCLUDED IN ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS), C.Y.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ABUTMENT REPAIRS
BRIDGE NO. 47-10275-1.02 L & R
FEDERAL BRIDGE ID NOS.
47102750007 AND 47102750008
I-275 OVER OLDHAM AVENUE
KNOX COUNTY
2026

BR-133-323

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PIN NO.: 083980.01
DESIGN BY: BRAD WARREN DATE: AUGUST 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: AUGUST 2025
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2025
CHECKED BY: FRANK BALE DATE: AUGUST 2025

SPECIAL NOTES FOR EPOXY INJECTION

UNLESS OTHERWISE NOTED, THE INTENT OF THIS SPECIFICATION IS FOR DESIGNATED CRACKS TO BE INJECTED THEIR FULL LENGTH AND DEPTH.

DESIGNATED CRACKS SHALL BE INJECTED WITH AN APPROVED EPOXY RESIN ADHESIVE FILLING ALL VOIDS FOR THE CRACK DEPTH OR THICKNESS OF THE MEMBER. THE EPOXY RESIN ADHESIVE SHALL BE ON THE CURRENT QUALIFIED PRODUCTS LIST MAINTAINED BY THE DIVISION OF MATERIALS AND TEST. ALL CRACKS SHALL BE INJECTED USING AN ADHESIVE SUITABLE FOR THE FIELD CONDITIONS (CRACK WIDTH, TEMPERATURE, HUMIDITY, ETC.) RECOMMENDED BY THE ADHESIVE MANUFACTURER AS SHOWN ON MATERIAL DATA SHEETS. FOLLOWING INJECTION, ALL INJECTION PORTS AND CAPPING MATERIAL SHALL BE REMOVED FROM EXPOSED SURFACES LEAVING THE SURFACE SMOOTH AND FLUSH WITH THE SURROUNDING CONCRETE SURFACES.

THE CONTRACTOR SHALL HAVE SUFFICIENT EXPERIENCE AND TRAINING TO PERFORM THE EPOXY INJECTION IN ACCORDANCE WITH THESE PLANS. PRIOR TO PERFORMING ANY WORK, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A WRITTEN PROCEDURE FOR PERFORMING THE EPOXY INJECTION. THE PROCEDURE SHALL DESCRIBE IN DETAIL HOW THE WORK WILL BE PERFORMED. THE PROCEDURE SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING INFORMATION.

- 1) DESCRIPTION OF EQUIPMENT.
 - A. THE INJECTION EQUIPMENT SHALL BE OF THE TYPE THAT MIXES ADHESIVE COMPONENTS AT THE INJECTION HEAD.
 - B. THE INJECTION EQUIPMENT SHALL BE CAPABLE OF DISCHARGING MIXED ADHESIVE AT ANY PRESSURE UP TO 300 PSI. THE INJECTION EQUIPMENT SHALL BE EQUIPPED WITH GAUGES WHICH CAN MEASURE THE INJECTION PRESSURE AND VOLUME.
- 2) EQUIPMENT CALIBRATION PROCEDURES AND SCHEDULE.
- 3) MATERIALS TO BE USED (INCLUDING MANUFACTURER DATA SHEETS)
 - A. CAPPING MATERIAL
 - B. EPOXY ADHESIVE (TYPE TO BE APPROPRIATE FOR CRACK SIZES TO BE INJECTED).
- 4) PORT SPACING
 - A. PORT SPACING SHALL NOT BE LESS THAN THE THICKNESS OF THE CONCRETE IN THAT LOCATION.
- 5) INJECTION SEQUENCE
 - A. INJECTION SHALL PROCEED FROM LOWER END OF CRACK ALONG ADJACENT PARTS.
 - B. SKIPPING OF PORTS DURING INJECTION SHALL NOT BE ALLOWED.

THE CONTRACTOR SHALL HAVE THE MANUFACTURER'S INSTRUCTIONS FOR PROPORTIONING AND MIXING AVAILABLE AT THE JOB SITE AT ALL TIMES AND SHALL ENSURE THAT THE EQUIPMENT IS SUPPLYING THE MIXED ADHESIVE IN THE CORRECT PROPORTIONS.

TO ENSURE PROPER MIXING AND PROPORTIONING, SAMPLES SHALL BE TAKEN FROM THE INJECTOR HEAD AT THE START OF EACH WORKDAY AND EACH TIME THE ADHESIVE RESERVOIRS ARE REFILLED. THE SAMPLES SHALL BE IN A TEST CUP. THE SAMPLE SHALL BE MONITORED TO ENSURE THAT THE CURE TIME IS IN COMPLIANCE WITH THE MANUFACTURER'S DATA SHEETS. IF THE SAMPLES DO NOT CURE IN THE SPECIFIED TIME THEN THE EQUIPMENT USED TO PRODUCE THE SAMPLE SHALL NOT BE USED UNTIL THE PROBLEM IS CORRECTED.

CORE SAMPLES SHALL BE TAKEN AS VERIFICATION OF THE QUALITY OF WORK. THE CONTRACTOR SHALL TAKE ONE (1) ONE (1) INCH DIAMETER (FULL DEPTH OF CONCRETE AT LOCATION CORED) CORE SAMPLES STARTING WITH THE FIRST REPAIR LOCATION THEN EVERY THIRD REPAIR LOCATION AFTERWARDS. WORK SHALL NOT PROCEED UNTIL THE CORE SAMPLE IS TAKEN AND ACCEPTED. ALL CORE SAMPLES AND HOLES SHALL BE INDEXED FOR FUTURE REFERENCE. THE ENGINEER SHALL DESIGNATE ALL LOCATIONS TO BE CORED. IF ANY CORES SHOW UNACCEPTABLE RESULTS, ALL WORK SHALL BE STOPPED UNTIL THE CONTRACTOR SUBMITS A PROPOSAL FOR CORRECTING UNACCEPTABLE WORK.

THE INITIAL CORE WILL ALSO SERVE TO QUALIFY THE FOREMAN FOR THIS WORK. IF AT ANY TIME A NEW FOREMAN IS USED, HE SHALL BE QUALIFIED WITH A CORE SAMPLE.

THE CONTRACTOR, AT HIS EXPENSE, SHALL REPAIR ALL CORE HOLES WITH AN APPROVED CEMENTITIOUS PATCHING MATERIAL.

CORE SAMPLES SHALL BE VISUALLY INSPECTED TO CONFIRM THAT CRACKS ARE COMPLETELY FILLED WITH ADHESIVE. ANY CORE HAVING LESS THAN 95% OF THEM CRACK FILLED WITH ADHESIVE SHALL BE CONSIDERED UNACCEPTABLE AND BE REJECTED.

CORE SAMPLES SHALL BE TESTED FOR BOND STRENGTH. SAMPLES MAY BE FRACTURED BY HAMMER BLOW TO CRACK AREA OR THROWN AT A HARD SURFACE. IF ADHESIVE FAILURE OCCURS BEFORE CONCRETE FAILURE, THE CORE SHALL BE CONSIDERED UNACCEPTABLE AND REJECTED.

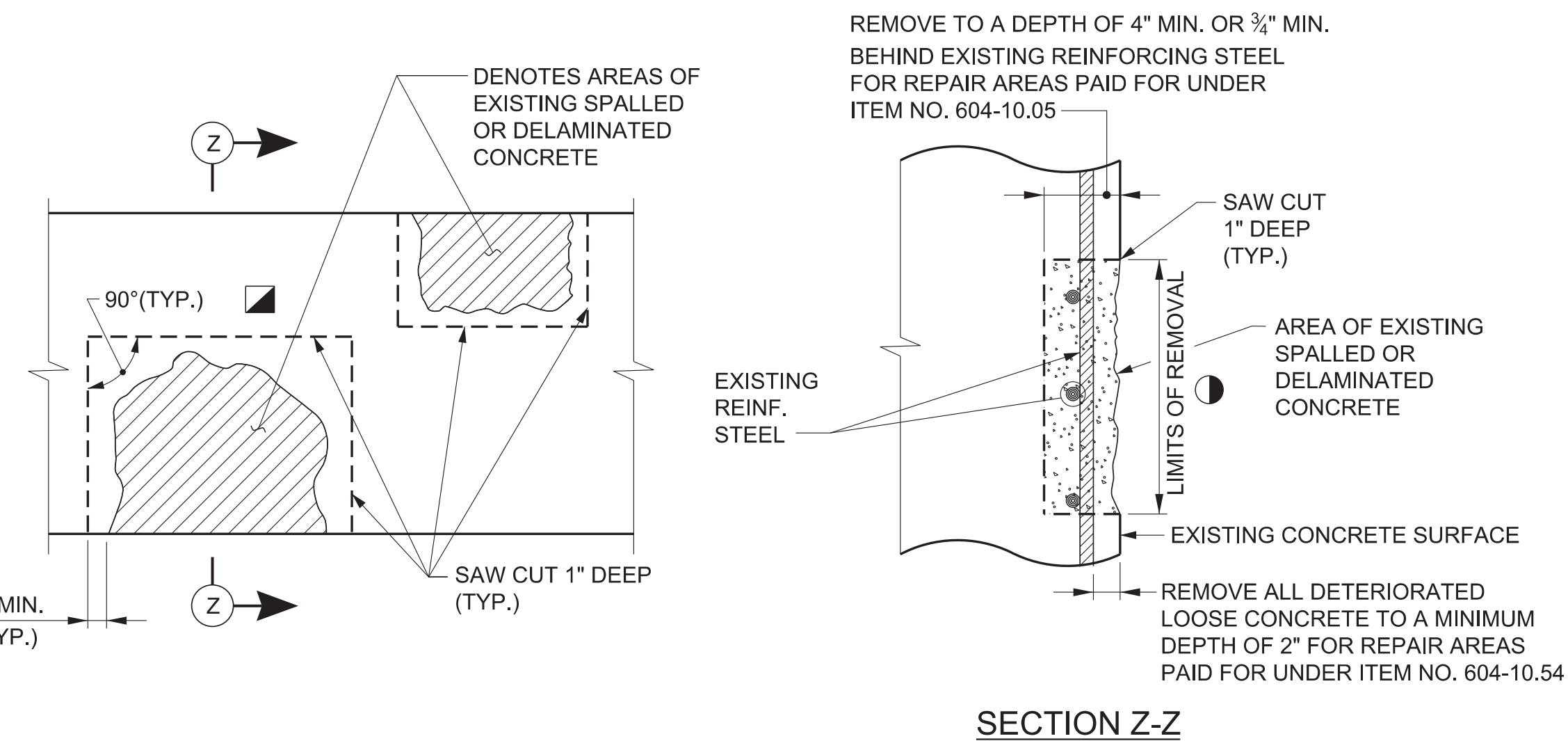
PAYMENT FOR EPOXY INJECTION CRACK REPAIR SHALL BE MADE UNDER ITEM NUMBERS 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F. AND 604-10.58, EPOXY INJECTION (INJECTION), GAL.

PRICE BID FOR ITEM NUMBER 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F. SHALL INCLUDE COST OF ALL LABOR AND MATERIALS (EXCEPT ADHESIVE) FOR GRINDING FOR SURFACE PREPARATION, CRACK PREPARATION, CAPPING, INJECTION OF ADHESIVE, ALL SAMPLES AND TESTING, REMOVAL OF CAPPING MATERIAL AND PORTS, AND OTHER INCIDENTALS. CRACKS SHALL BE MEASURED FOR PAYMENT ALONG THE LENGTH OF THE VISIBLE SURFACE CRACK.

PRICE BID FOR ITEM 604-10.58, EPOXY INJECTION (INJECTION), GAL., SHALL INCLUDE COST FOR ADHESIVE MATERIAL INJECTED ONLY.

NO PAYMENT SHALL BE MADE FOR REWORK DEEMED NECESSARY BY FAILURE OF ADHESIVE SAMPLES OR CORE SAMPLES.

ALL WORK INCLUDING SAMPLING AND TESTING SHALL BE IN THE PRESENCE OF THE ENGINEER OR HIS REPRESENTATIVE OR CONTRACT INSPECTORS. ANY WORK DONE WITHOUT INSPECTORS PRESENT SHALL NOT BE PAID FOR. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH WEEKLY SCHEDULES OF WORK TO BE PERFORMED. SCHEDULES SHALL BE SUBMITTED AT LEAST THREE (3) DAYS IN ADVANCE OF WORK TO BE DONE. THE ENGINEER SHALL BE NOTIFIED OF ANY CHANGE IN THE SCHEDULE A MINIMUM OF TWENTY-FOUR (24) HOURS IN ADVANCE OF CHANGE.



DETAILS SHOWING AREAS OF EXISTING SPALLED OR DELAMINATED CONCRETE SURFACES TO BE REMOVED AND REPAIRED

- - DENOTES LIMITS AND LOCATION OF REPAIRS TO BE DESIGNATED BY THE ENGINEER
- ◻ - DENOTES SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS

NOTES

EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED OR DELAMINATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED EXISTING REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE AND INCORPORATED INTO THE NEW CONSTRUCTION. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.

THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALLED OR DELAMINATED AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE HIGH EARLY STRENGTH CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13, SECTION B, MATERIAL QPL 13.009.

PNEUMATICALLY PLACED CONCRETE IS NOT ALLOWED.

REMOVE TO A DEPTH OF 4" MIN. OR 3/4" MIN. BEHIND EXISTING REINFORCING STEEL FOR REPAIR AREAS PAID FOR UNDER ITEM NO. 604-10.05

SAW CUT 1" DEEP (TYP.)
 LIMITS OF REMOVAL
 AREA OF EXISTING SPALLED OR DELAMINATED CONCRETE
 EXISTING REINF. STEEL
 EXISTING CONCRETE SURFACE
 REMOVE ALL DETERIORATED LOOSE CONCRETE TO A MINIMUM DEPTH OF 2" FOR REPAIR AREAS PAID FOR UNDER ITEM NO. 604-10.54

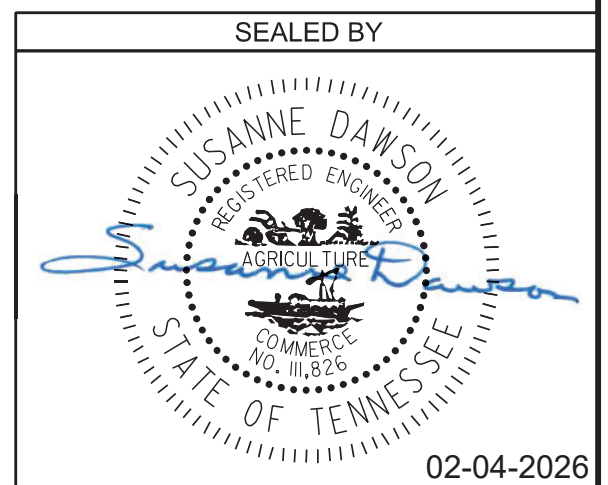
SECTION Z-Z

COST OF SAW CUTTING, REMOVING SPALLED OR DELAMINATED CONCRETE, CLEANING, PATCHING MATERIAL, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN TO BE INCLUDED IN ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F. OR ITEM NO. 604-10.05, CONCRETE, S.F.

THE ENGINEER SHALL DESIGNATE ALL SPALLED OR DELAMINATED CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND 604-10.54 MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER HAND DRIVEN TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:
 1. PNEUMATIC HAMMERS HEAVIER THAN THE 35 LB. CLASS SHALL NOT BE USED.
 2. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL.

PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 CONCRETE REPAIR DETAILS
 BRIDGE NO. 47-10275-1.02 L & R
 FEDERAL BRIDGE ID NOS.
 47102750007 AND 47102750008
 I-275 OVER OLDHAM AVENUE

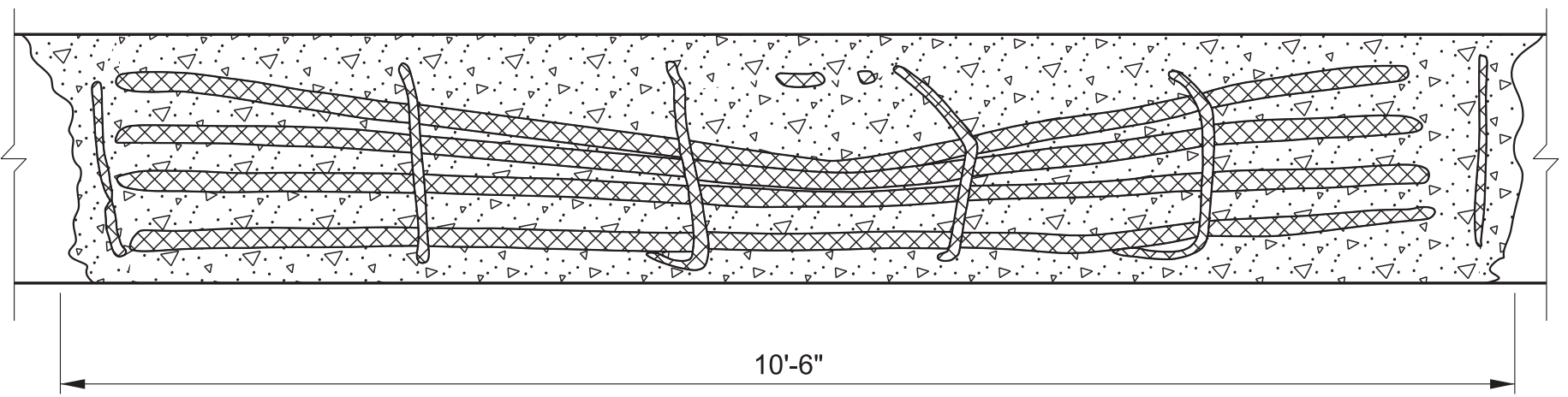
KNOX COUNTY
 2026

BR-133-327

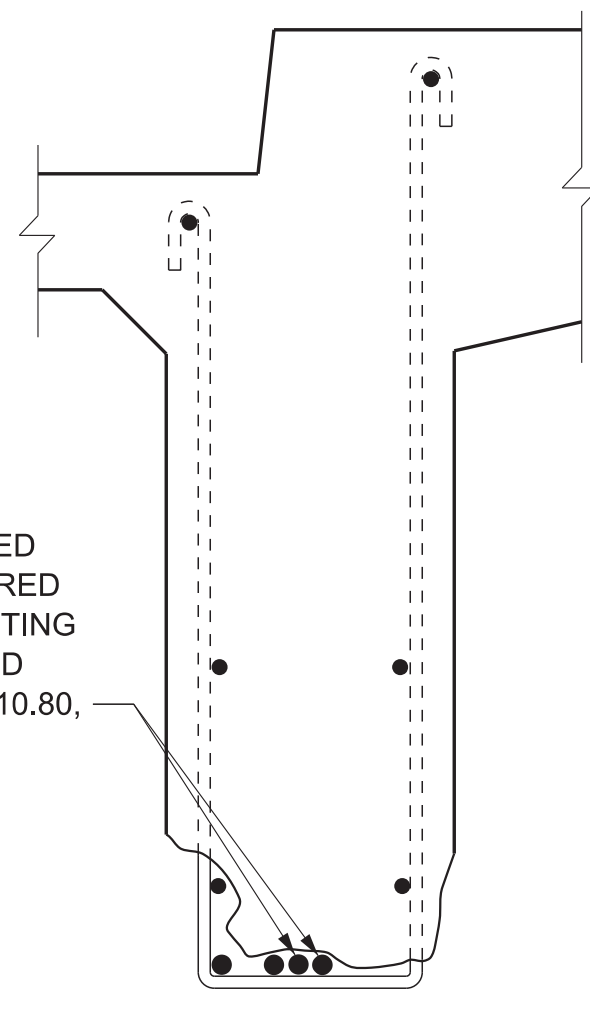
PIN NO.:	083980.01	DATE:	AUGUST 2025
DESIGN BY:	BRAD WARREN	DATE:	AUGUST 2025
DRAWN BY:	T. PELOW/D. PICKEL	DATE:	AUGUST 2025
SUPERVISED BY:	SUSANNE DAWSON	DATE:	AUGUST 2025
CHECKED BY:	FRANK BALE	DATE:	AUGUST 2025

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PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

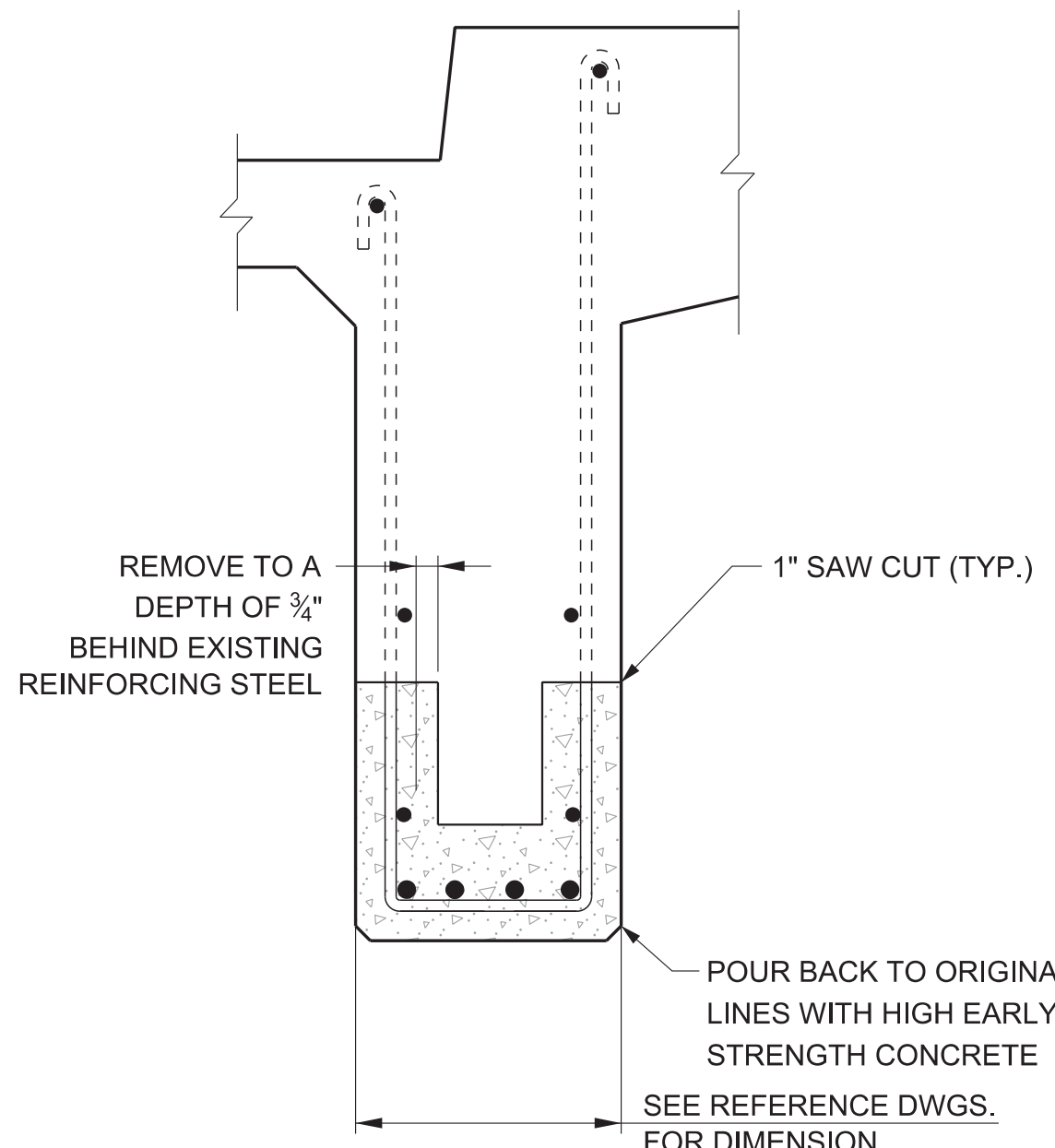


EXISTING BOTTOM OF BEAM COLLISION DAMAGE
FOR AREA DENOTED BY [Pattern] ON BEAM 6R ON SHEET BR-133-322



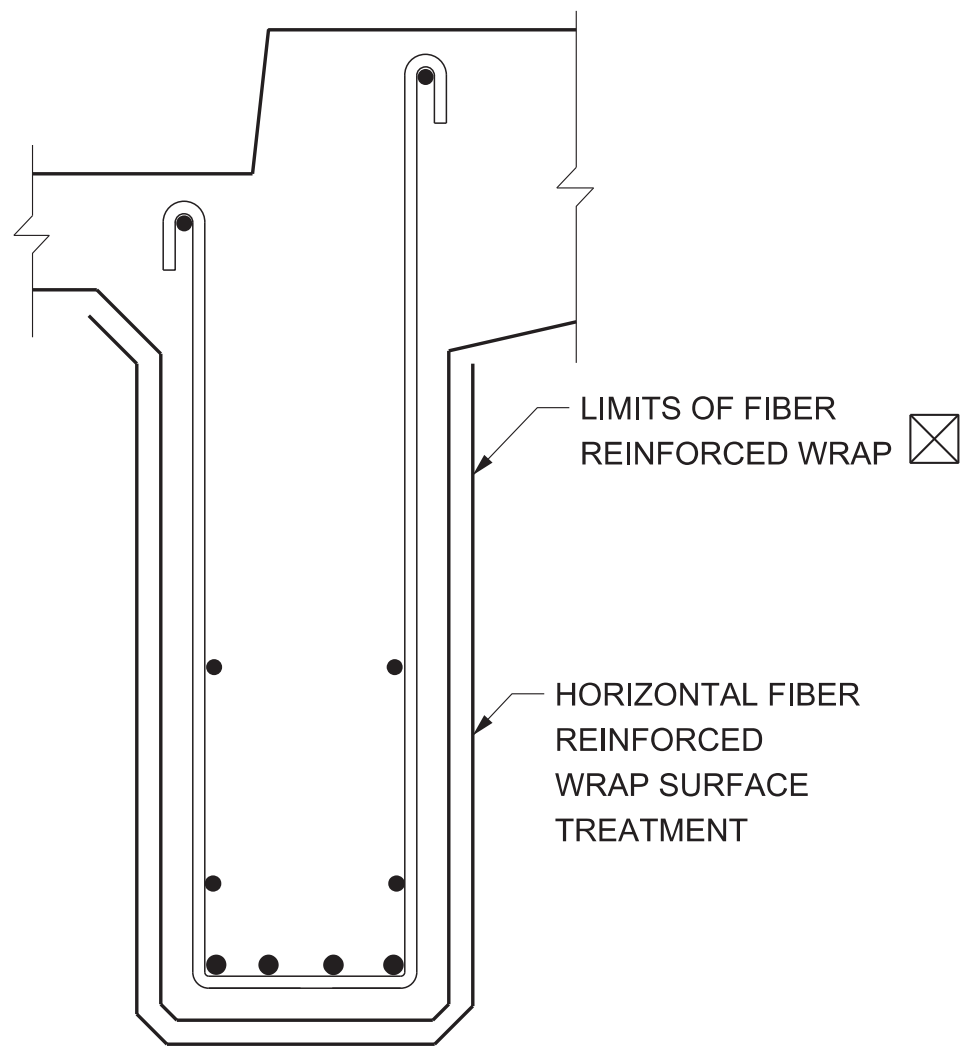
SECTION A-A COLLISION DAMAGE

REMOVE AND REPLACE DAMAGED NO. 11 BARS. ALL COSTS REQUIRED TO REMOVE AND REPLACE EXISTING NO. 11 BARS SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.80, BRIDGE REPAIRS, L.S.



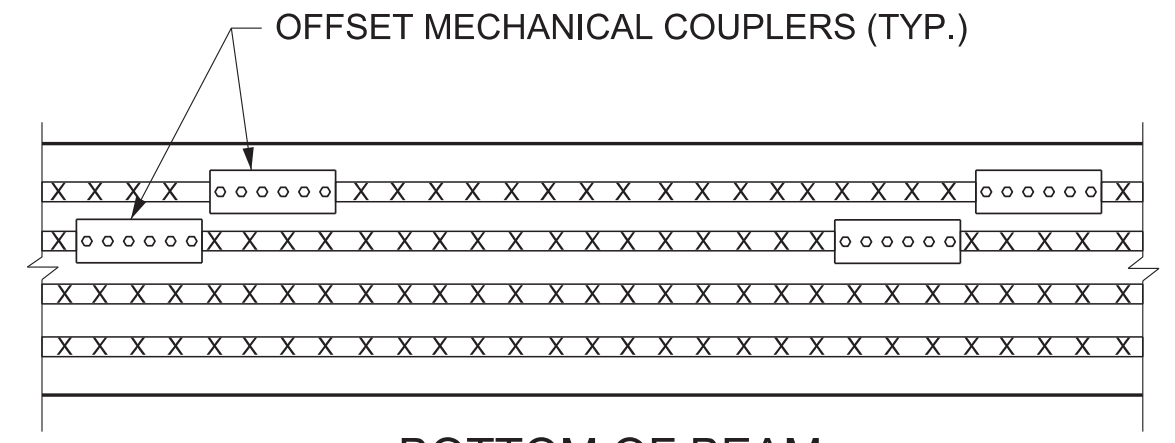
BEAM REPAIR DETAIL

FOR AREA DENOTED BY [Pattern] ON BEAM 6R ON SHEET BR-133-322



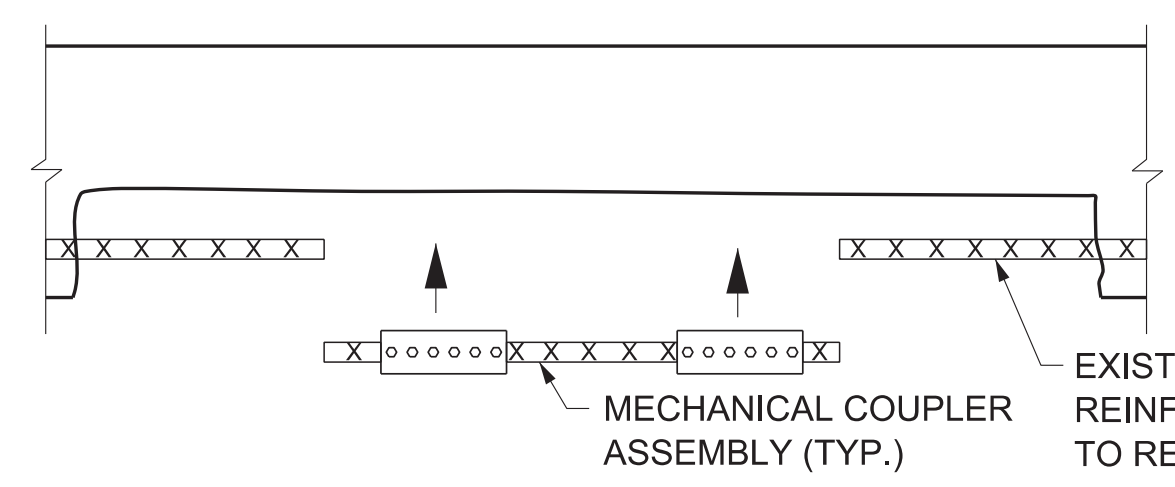
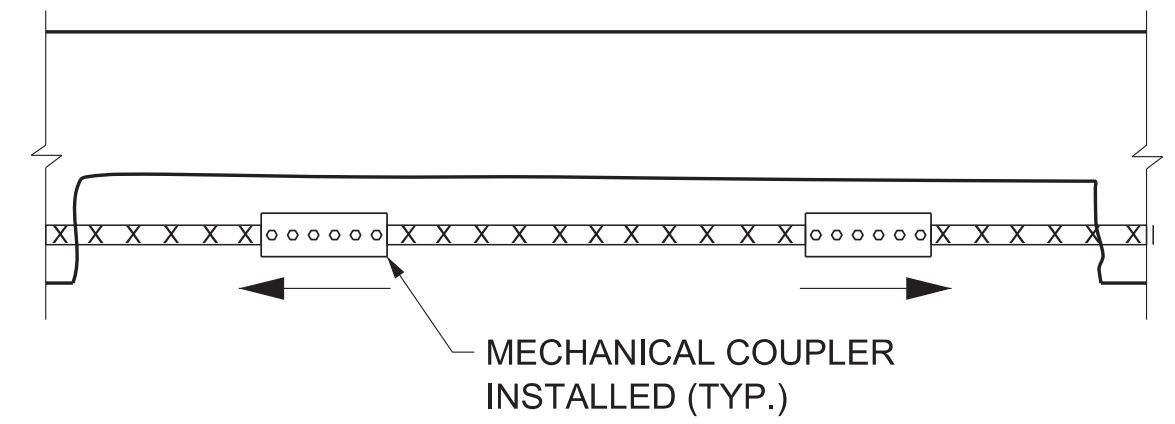
SECTION A-A REPAIRED BEAM

SEE FIBER WRAP NOTES BELOW FOR ADDITIONAL LIMITS.



BOTTOM OF BEAM MECHANICAL COUPLERS INSTALLED

NOTE: RECORDS INDICATE MECHANICAL COUPLERS ARE IN PLACE FROM A PREVIOUS REPAIR. REMOVE EXISTING COUPLERS IF IN CONFLICT BEFORE INSTALLING NEW.



ELEVATIONS SHOWING TYPICAL REPLACEMENT OF DAMAGED REBAR IN EXISTING CONCRETE

FOR AREA DENOTED BY [Pattern] ON BEAM 6R ON SHEET BR-133-322

SPECIAL NOTE REGARDING BEAM REPAIR: TO REPAIR THE TWO OUTERMOST REINFORCING BARS, REMOVE DEFORMED BAR AND REPLACE IN KIND JOINING EXISTING REINFORCING WITH A MECHANICAL COUPLER DESIGNED TO TRANSFER FULL TENSION ACROSS THE SPLICE, SUCH AS DAYTON SUPERIOR BAR LOCK COUPLER SYSTEM, OR TDOT APPROVED EQUAL. SPLICES SHALL BE STAGGERED TO AVOID CONFLICT WITH ADJACENT MECHANICAL COUPLERS.

COUPLER DETAILS

THE COST OF MECHANICAL THREADED CONNECTORS AND COUPLER BARS TO BE INCLUDED IN THE UNIT PRICE BID FOR ITEM NO. 604-10.80, BRIDGE REPAIRS, L.S.

ESTIMATED QUANTITIES

LOCATION OF REPAIR	ITEM NO. 604-10.83 APPROX. REPAIR AREAS (S.F.)
BEAM 6R - RIGHT BRIDGE	203

NOTES:

THE COST OF REMOVING PORTION OF THE EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, AND ALL MISCELLANEOUS MATERIAL NECESSARY TO COMPLETE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.80, BRIDGE REPAIRS, L.S.

CONTRACTOR TO TAKE EXTREME CARE WHEN REMOVING DELAMINATED CONCRETE SO AS NOT TO DAMAGE EXISTING REINFORCING STEEL. ALL EXISTING REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE PLACING NEW CONCRETE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING BEAM REPAIRS. DETAILS OF ANY TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE THE BEGINNING OF ANY BEAM REPAIR. COST TO BE INCLUDED IN ITEM NO. 604-10.80, BRIDGE REPAIRS, L.S.

FOR ADDITIONAL DETAILS AND NOTES REGARDING SPALL REPAIR, SEE DWG. NO. BR-133-327.

NOTES (CONT.):

REPAIRS DENOTED AS ITEM NO. 604-10.42, CONCRETE REPAIRS ON DWG. NO. BR-133-322 SHALL BE REPAIRED USING BEAM REPAIR DETAILS ON THIS SHEET. NO REBAR WILL BE REQUIRED TO BE REMOVED AND REPLACED. NO FIBER WRAP SHALL BE APPLIED TO THESE REPAIRS.

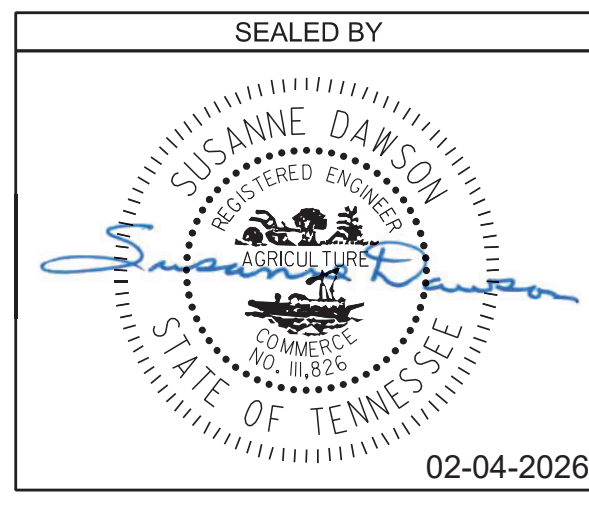
COMPOSITE FIBER WRAP NOTES:

NEW FIBER REINFORCED WRAP SHALL BE A UNIDIRECTIONAL CARBON FIBER WRAP MEETING THE REQUIRED MATERIALS PROPERTIES SHOWN IN SPECIAL PROVISION SP604FRP. PRIOR TO APPLICATION OF FIBER REINFORCED WRAP, ALL BEAM REPAIRS SHALL BE COMPLETED, AND CONCRETE SURFACES SHALL BE PREPARED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS AND SPECIAL PROVISION SP604FRP. FIBER WRAP SHALL BE INSTALLED IN STRICT ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS AND TDOT SPECIAL PROVISION SP604FRP.

FIBER REINFORCED WRAP SHALL BE APPLIED LONGITUDINALLY ON BEAM WITH FIBER ORIENTATION PARALLEL TO BEAM CENTERLINE. FIBER WRAP LIMITS SHALL ENCOMPASS THE ROADWAY WIDTH ALONG THE BEAM NOT INCLUDING SHOULDERS AND EXTEND 1'-0" MIN. TO 3'-0" MAX. PAST THE EDGE OF ROADWAY.

ALL COSTS OF LABOR, MATERIALS, AND INCIDENTALS NECESSARY TO PLACE CARBON FIBER EPOXY WRAP SHALL BE PAID FOR UNDER ITEM NO. 604-10.83, COMPOSITE FIBER ENCASEMENT, S.F.

BEAM WILL BE FINISHED WITH AN APPLIED TEXTURE FINISH. SEE DWG. NO. BR-133-320 FOR DETAILS.

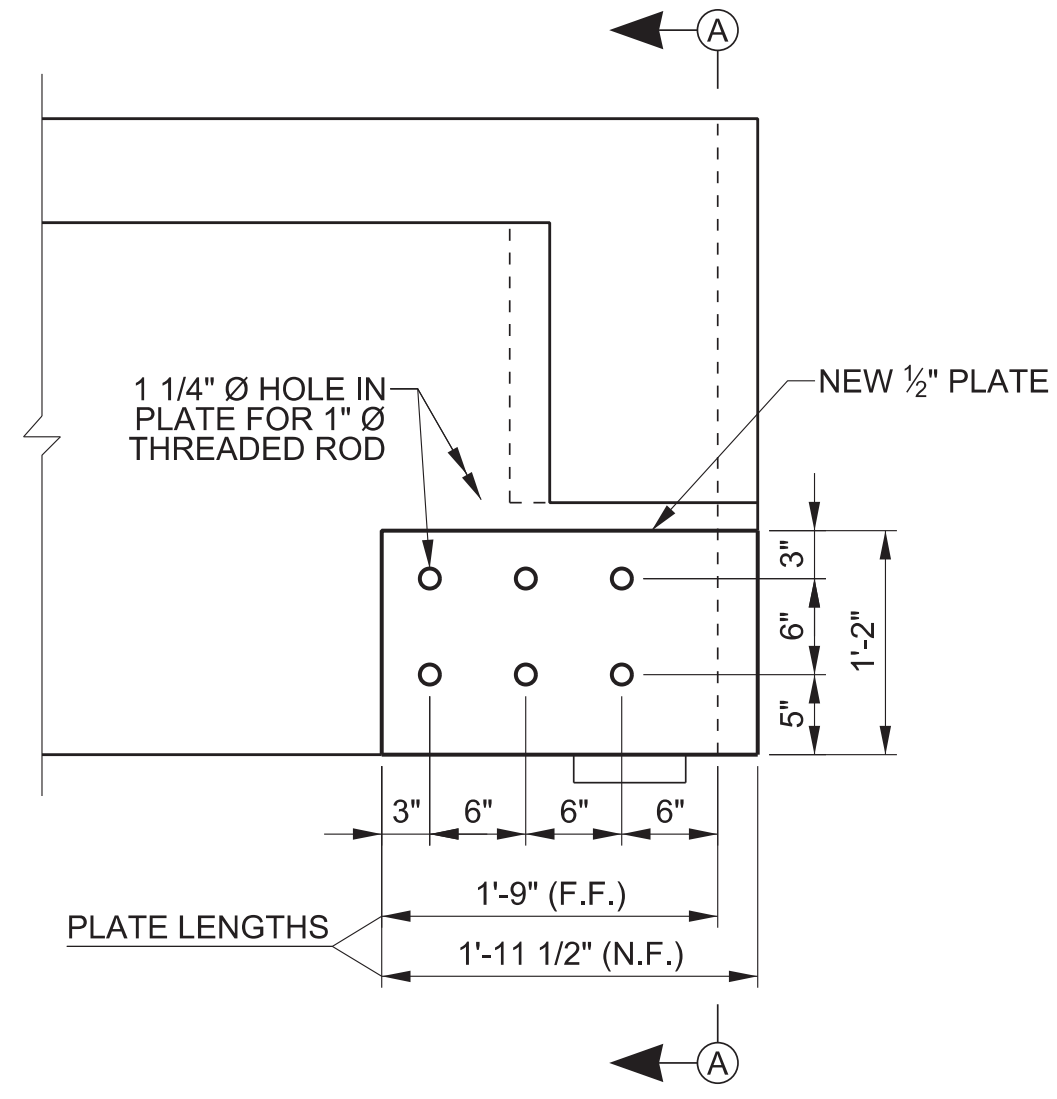


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BEAM COLLISION REPAIR DETAILS
BRIDGE NO. 47-10275-1.02 L & R
FEDERAL BRIDGE ID NOS.
47102750007 AND 47102750008
I-275 OVER OLDHAM AVENUE
KNOX COUNTY
2026

BR-133-328

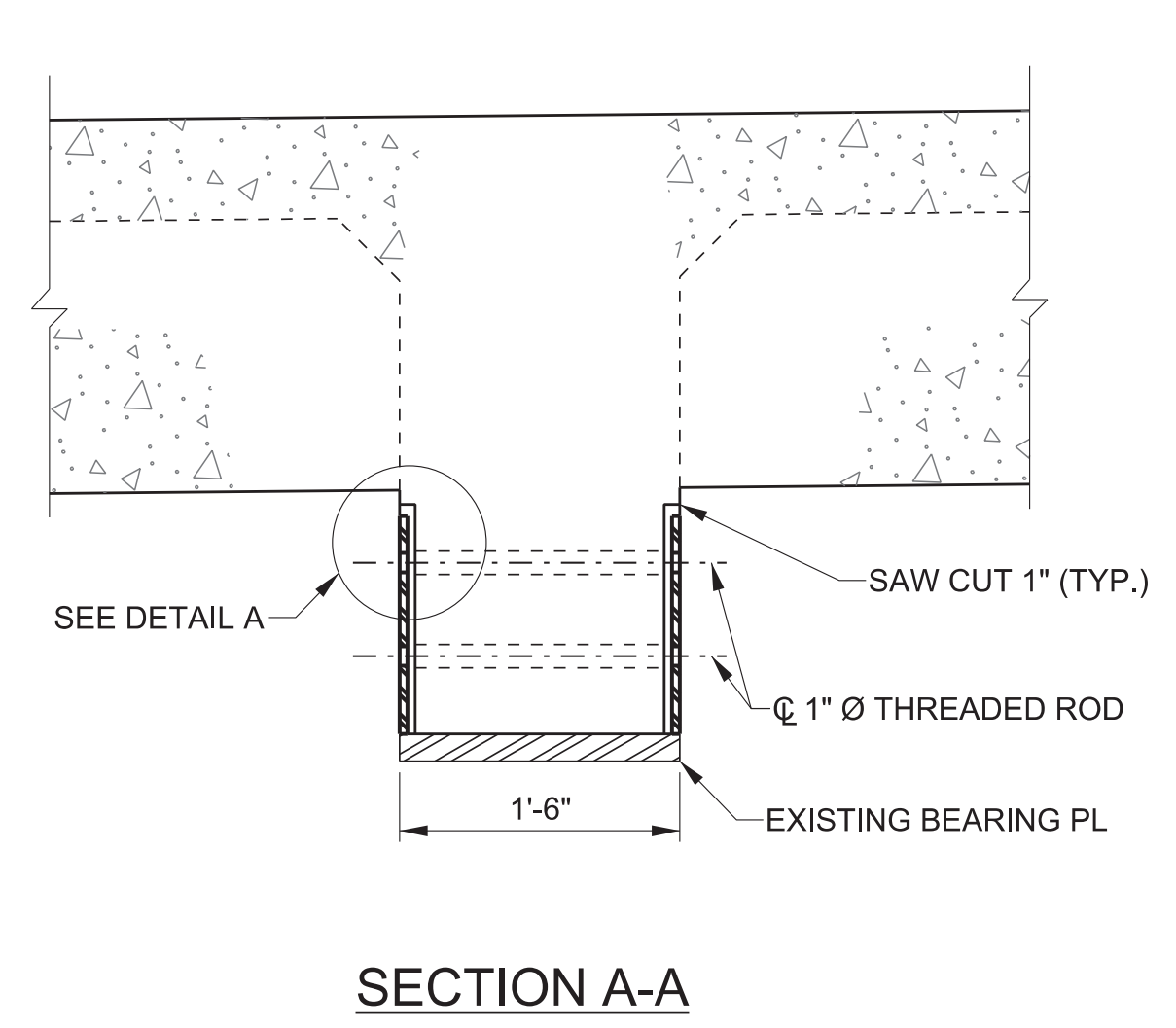
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PROJECT NO.	YEAR	SHEET NO.	
471275-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

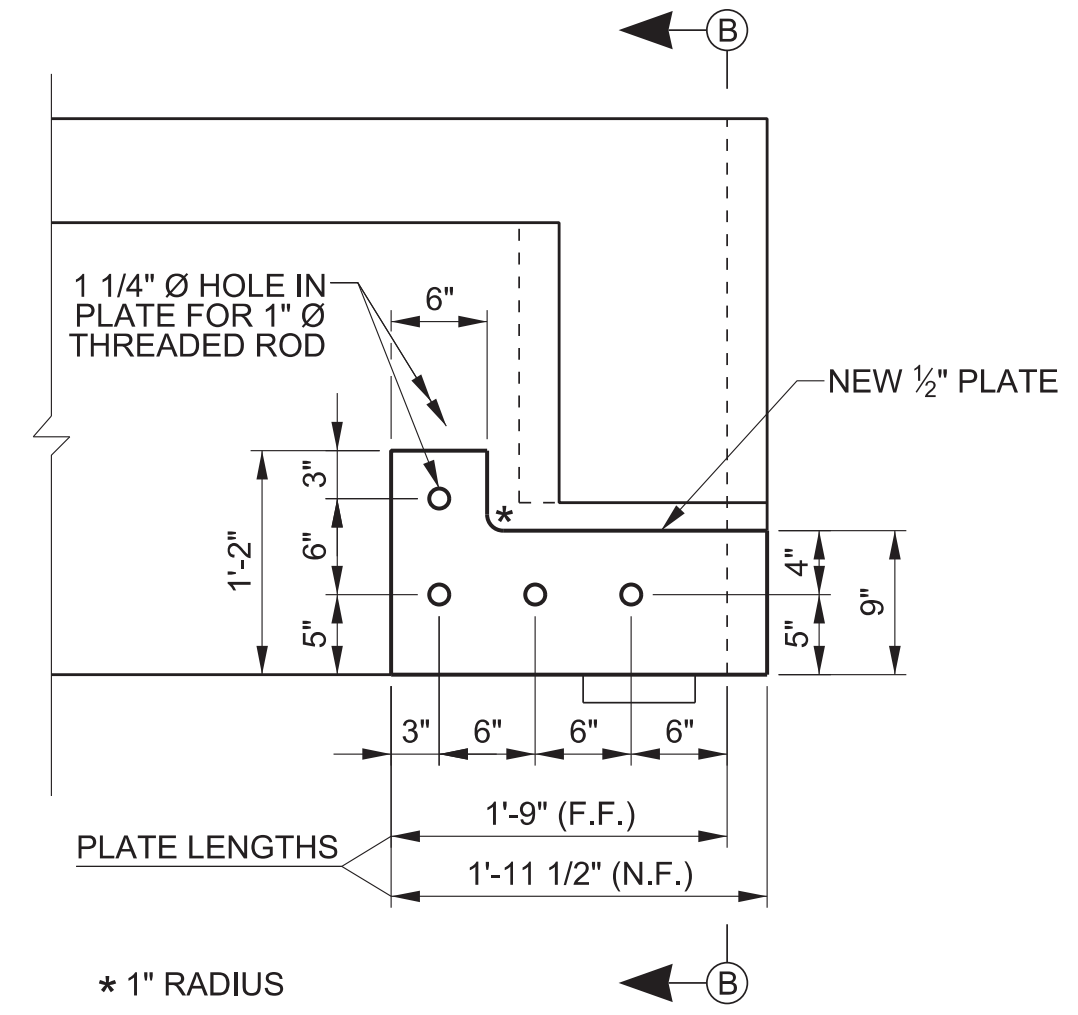


ELEVATION OF REPAIR TYPE A

TO BE USED FOR SPANS 2 AND 3
(6 TOTAL LOCATIONS)

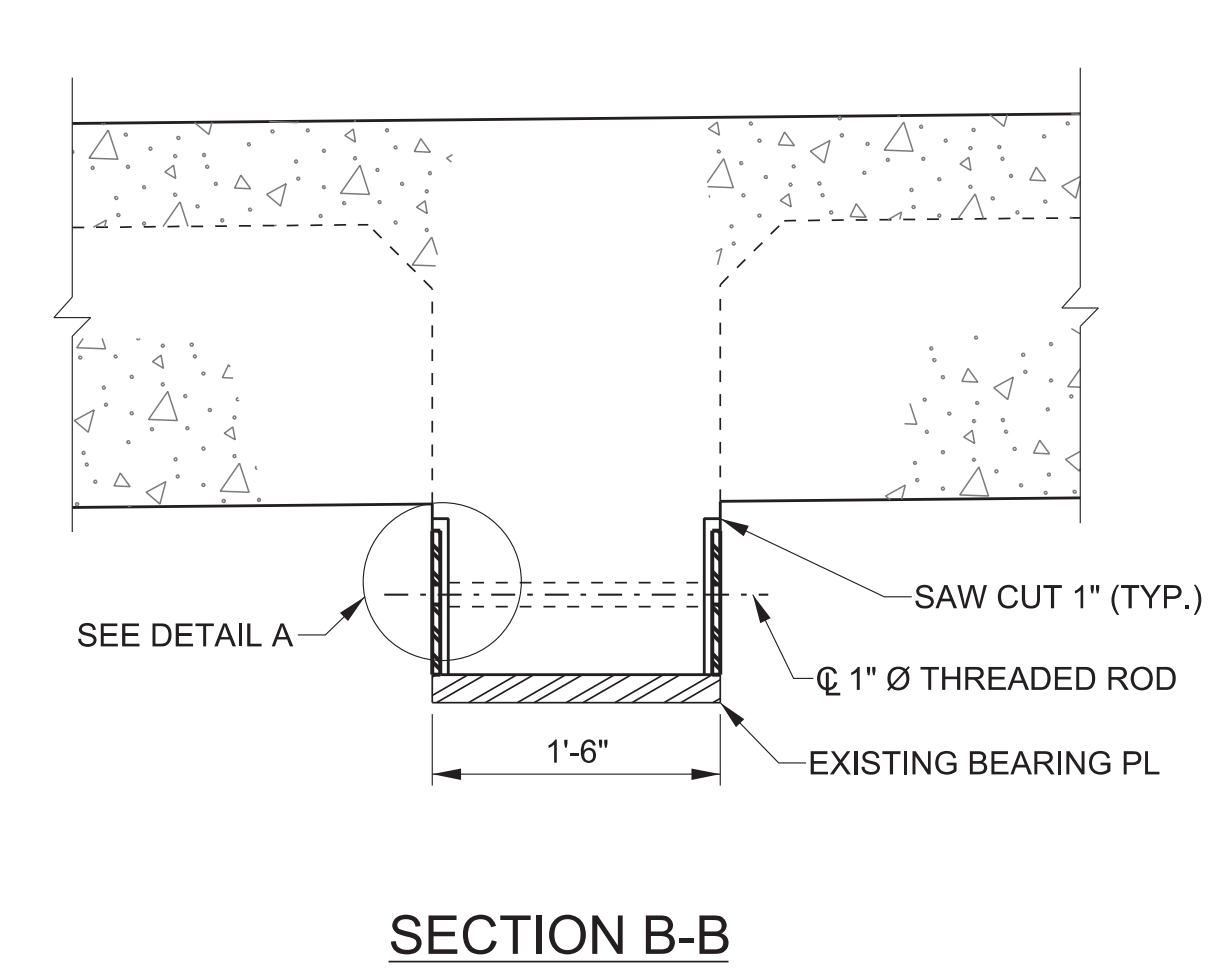


SECTION A-A



ELEVATION OF REPAIR TYPE B

TO BE USED FOR SPAN 4
(1 TOTAL LOCATION)



SECTION B-B

NOTE: THE CONTRACTOR SHALL SUPPORT END OF BEAMS DURING REPAIRS. SUPPORT SYSTEM SHALL BE CAPABLE OF CARRYING THE FOLLOWING UNFACTORED LOADS (PER BEAM):

- SPAN 4: DEAD LOAD = 30 KIPS
LIVE LOAD W/ IMPACT = 60 KIPS
- SPANS 2 & 3: DEAD LOAD = 40 KIPS
LIVE LOAD W/ IMPACT = 65 KIPS

CONTRACTOR SHALL SUBMIT SUPPORT DETAILS TO THE ENGINEER FOR APPROVAL. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE STABILITY OF THE STRUCTURE DURING REPAIRS. ALL COST FOR SUPPORT SYSTEM, INCLUDING MATERIALS AND LABOR SHALL BE PAIED FOR UNDER ITEM NO. 604-10.26, BRACING, EACH.

SCOPE OF WORK FOR BEAM END REPAIRS

1. SAW CUT 1" AROUND PERIMETER OF NEW PLATE.
2. REMOVE EXISTING CONCRETE IN REPAIR AREA TO A DEPTH OF 1'.
3. REPAIR ANY EXISTING DAMAGED AREAS ON THE BEAM ENDS (CRACKS, SPALLS, ETC.) AS DIRECTED BY THE ENGINEER TO THE FULL SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 602-10.32, STRUCTURAL STEEL (REPAIRS), LB.
4. DRILL 1 1/2" DIA. HOLES THROUGH EXISTING CONCRETE BEAM TO ACCOMMODATE 1" DIA. THREADED ROD. HOLES SHALL BE DRILLED WITH HIGH SPEED DRILL. THE DRILL BIT SHALL BE CAPABLE OF DRILLING THROUGH CONCRETE AND REINFORCING STEEL. AFTER DRILLING, FILL HOLES WITH APPROVED NON-SHRINK GROUT JUST PRIOR TO PLACING RODS. THE 1 1/4" DIA. HOLES FOR THE 1" DIA. RODS SHALL NOT BE DRILLED IN THE 1/2" PLATES UNTIL THE 1 1/2" DIA. HOLES HAVE BEEN DRILLED IN THE EXISTING CONCRETE BEAMS. SEE QUALIFIED PRODUCTS LIST 16.001 FOR NON-SHRINK GROUT.
5. THE NEW 1/2" PLATES SHALL BE PLACED FLUSH WITH THE ORIGINAL LINES OF THE EXISTING CONCRETE BEAMS AND SHALL HAVE GOOD CONTACT WITH EXISTING BEARING PLATES. THE 1/2" VOLD BETWEEN NEW PLATES AND CHIPPED SURFACES OF EXISTING CONCRETE BEAM SHALL BE FILLED WITH APPROVED NON-SHRINK GROUT.
6. AFTER NON-SHRINK GROUT HAS BEEN CURED, THE CONTRACTOR SHALL TIGHTEN NUTS TO A SNUG FIT.
7. PLACE AN APPROVED SEALANT AROUND EDGES OF STEEL TO PREVENT MOISTURE FROM GETTING BETWEEN PLATES AND BEAM. SEE QUALIFIED PRODUCTS LIST 05.010 FOR SEALANT.

NOTE: THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN REMOVING EXISTING CONCRETE SO AS NOT TO DAMAGE EXISTING REINFORCING STEEL.

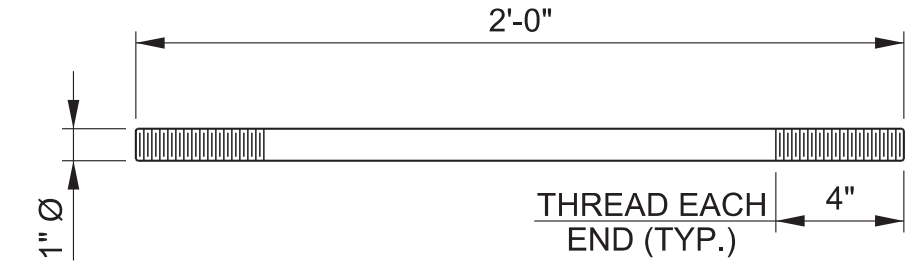
NOTE: ALL MATERIAL SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A123 UNLESS SHOWN OTHERWISE.

REPAIR ALL CRACKED, CHIPPED, OR SCRATCHED GALVANIZED STEEL MEMBERS BY "TOUCHING-UP" WITH AN APPROVED ZINC POWDER, WIRE, STICK, OR SPRAY MANUFACTURED ESPECIALLY FOR THIS PURPOSE. THE ZINC POWDER, WIRE, OR STICK SHALL BECOME COMPLETELY LIQUID AT A TEMPERATURE NO GREATER THAN 475 °F. THOROUGHLY CLEAN THE AREA TO BE REGALVANIZED, INCLUDING REMOVING SLAG ON WELDS, AND REPAIR ACCORDING TO THE RECOMMENDATIONS OF THE MANUFACTURER OF THE MATERIAL BEING USED.

NOTE: COST OF SAW CUTTING, REMOVING EXISTING CONCRETE, DRILLING, NON-SHRINK GROUT, STEEL PLATES, MACHINE THREADED RODS WITH HEX NUTS AND WASHERS, PAINTING, LABOR AND ANY MISCELLANEOUS MATERIALS OR INCIDENTALS NECESSARY TO CPMLETE THE REPAIRS SHOWN SHALL BE PAID FOR UNDER ITEM NO. 602-10.32, STRUCTURAL STEEL (REPAIRS), LB.

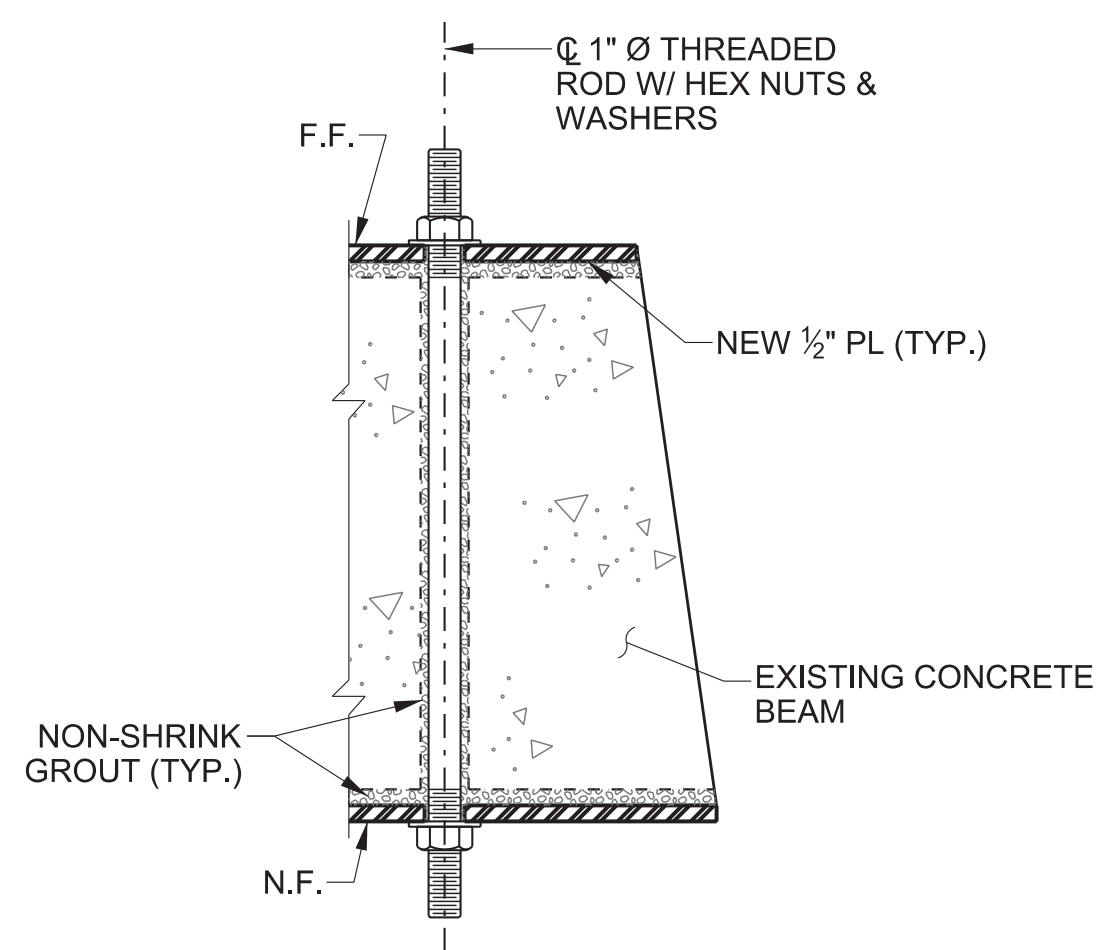
- STRUCTURAL STEEL SHALL BE ASTM A709 GRADE 50.
- HEX NUTS SHALL BE ASTM A563.
- HARDENED STEEL WASHERS SHALL BE ASTM F436.
- THREADED ROD SHALL BE ASTM F1554 GRADE 55.

N.F. - DENOTES NEAR FACE OF BEAM.
F.F. - DENOTES FAR FACE OF BEAM.

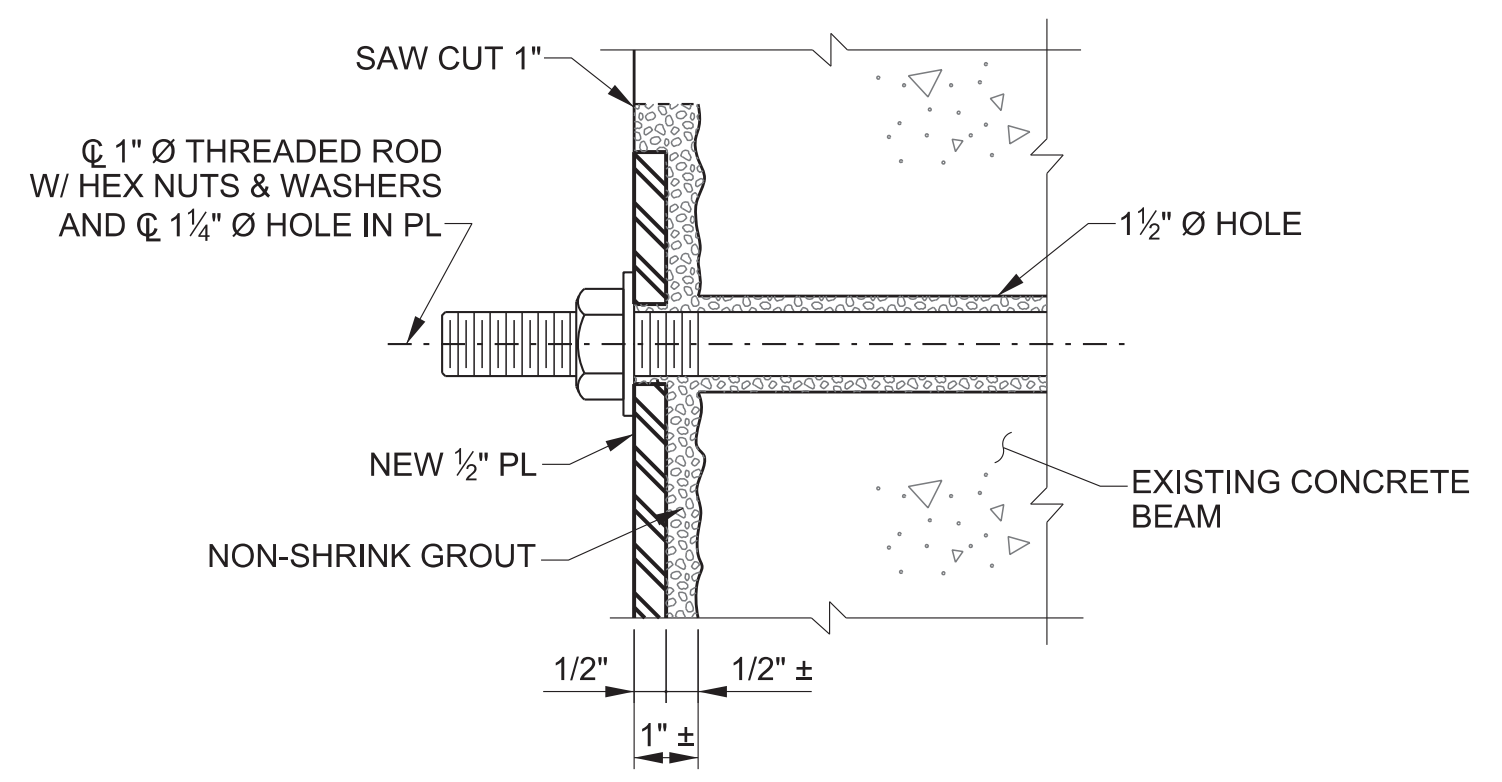


ROD DETAIL

1" Ø MACHINE THREADED ROD (ASTM F1554 GRADE 55)



TYPICAL PLAN OF BEAM END REPAIR

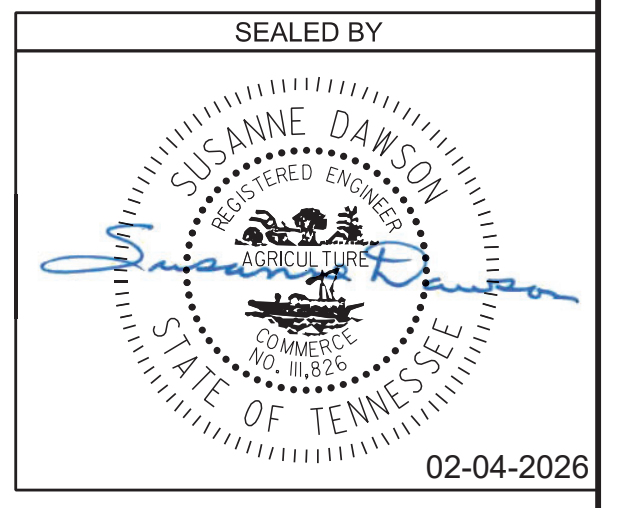


DETAIL A

NOTE: CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS BEFORE FABRICATION.

NOTE: THE NEW STEEL PLATES SHALL BE SHOP PRIMED BEFORE BEING DELIVERED TO THE BRIDGE SITE AND SHALL BE FIELD PAINTED AFTER INSTALLATION IS COMPLETE.

PIN NO.:	083980.01	DATE:	AUGUST 2025
DESIGN BY:	BRAD WARREN	DATE:	AUGUST 2025
DRAWN BY:	T. PELOWID, PICKEL	DATE:	AUGUST 2025
SUPERVISED BY:	SUSANNE DAWSON	DATE:	AUGUST 2025
CHECKED BY:	FRANK BALE	DATE:	AUGUST 2025



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BEAM END REPAIR DETAILS
BRIDGE NO. 47-10275-1.02 L & R
FEDERAL BRIDGE ID NOS.
47102750007 AND 47102750008
I-275 OVER OLDHAM AVENUE

KNOX COUNTY
2026

BR-133-329

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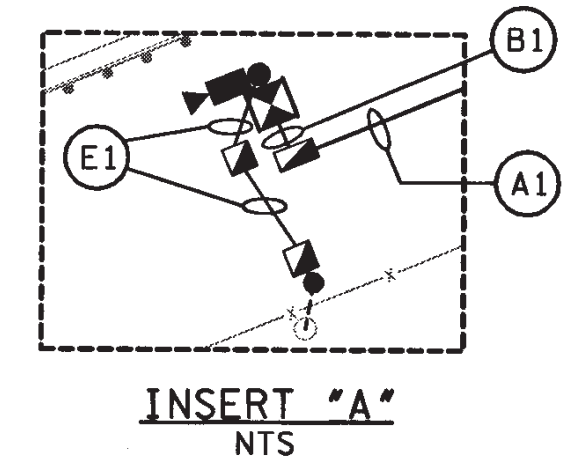
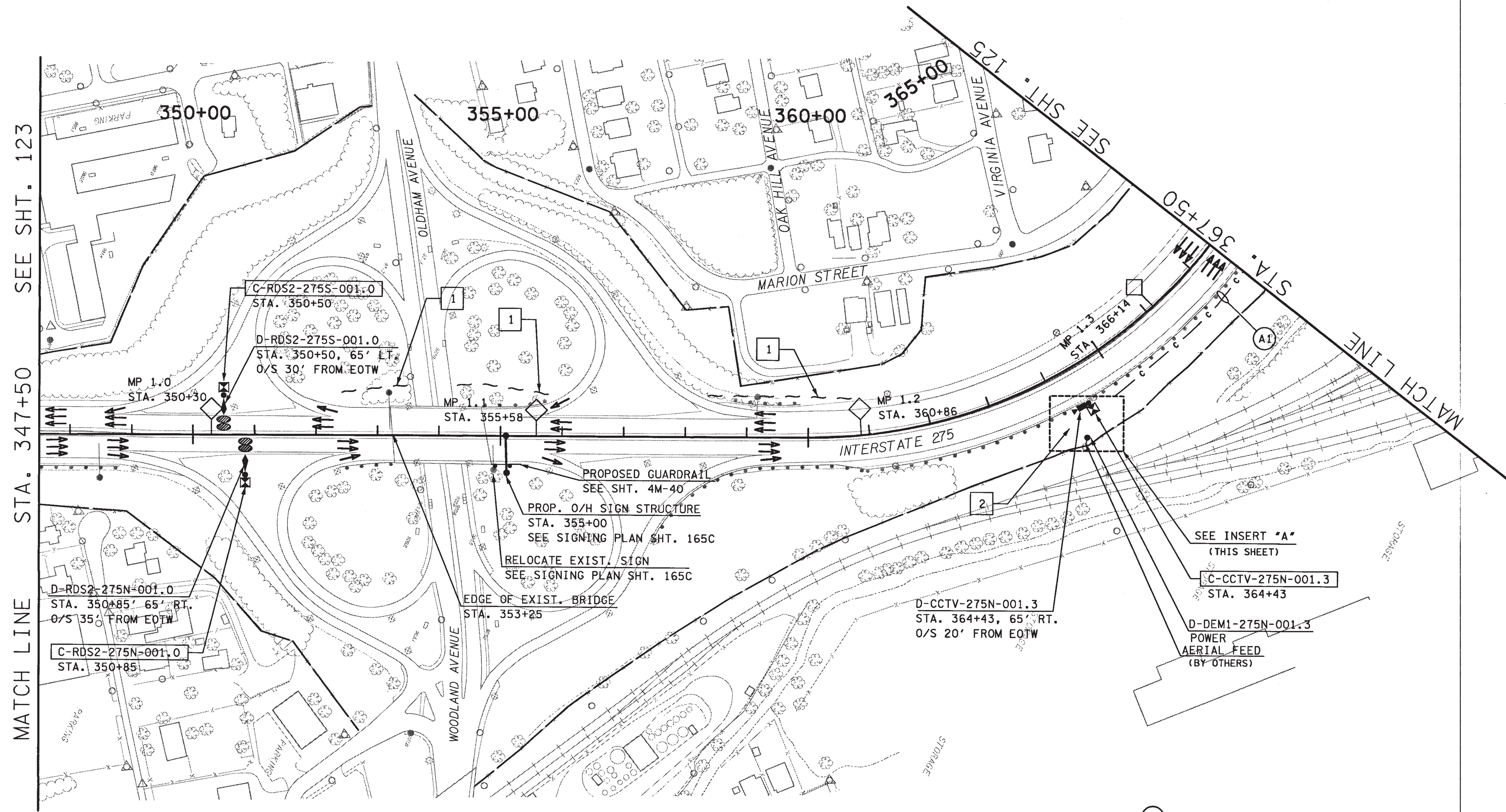
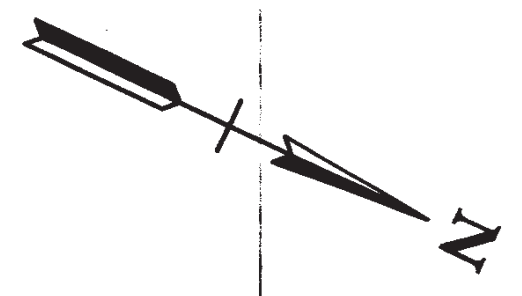
TENNESSEE D.O.T.
DESIGN DIVISION
FILE NO. **K-56**

CONDUIT AND CABLE SCHEDULE						
ITEM	RUN				B1	* E1
	1	2	3	4		
48 SMFO CBL	1				1	
#4 AWG CBL						3
CONDUIT SIZE & TYPE	4" PVC MD		2" RGS		2" PVC	

* CONDUIT INCLUDES 1 - #6 AWG. GROUNDING CONDUCTOR FOR BONDING THE SYSTEM

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2002	IM-098-1(7)	124

- NOTES:
- 1 SELECTIVE TREE AND VEGETATION TRIMMING, REMOVAL, AND CLEAN-UP REQUIRED. SEE GENERAL NOTES.
 - 2 CLEARING AND GRUBBING REQUIRED FOR DEVICE, CABINET, DEMARCATION SITE, AND CONDUIT ROUTE INSTALLATION AND FUTURE ACCESS.

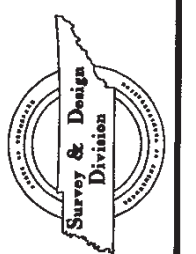


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

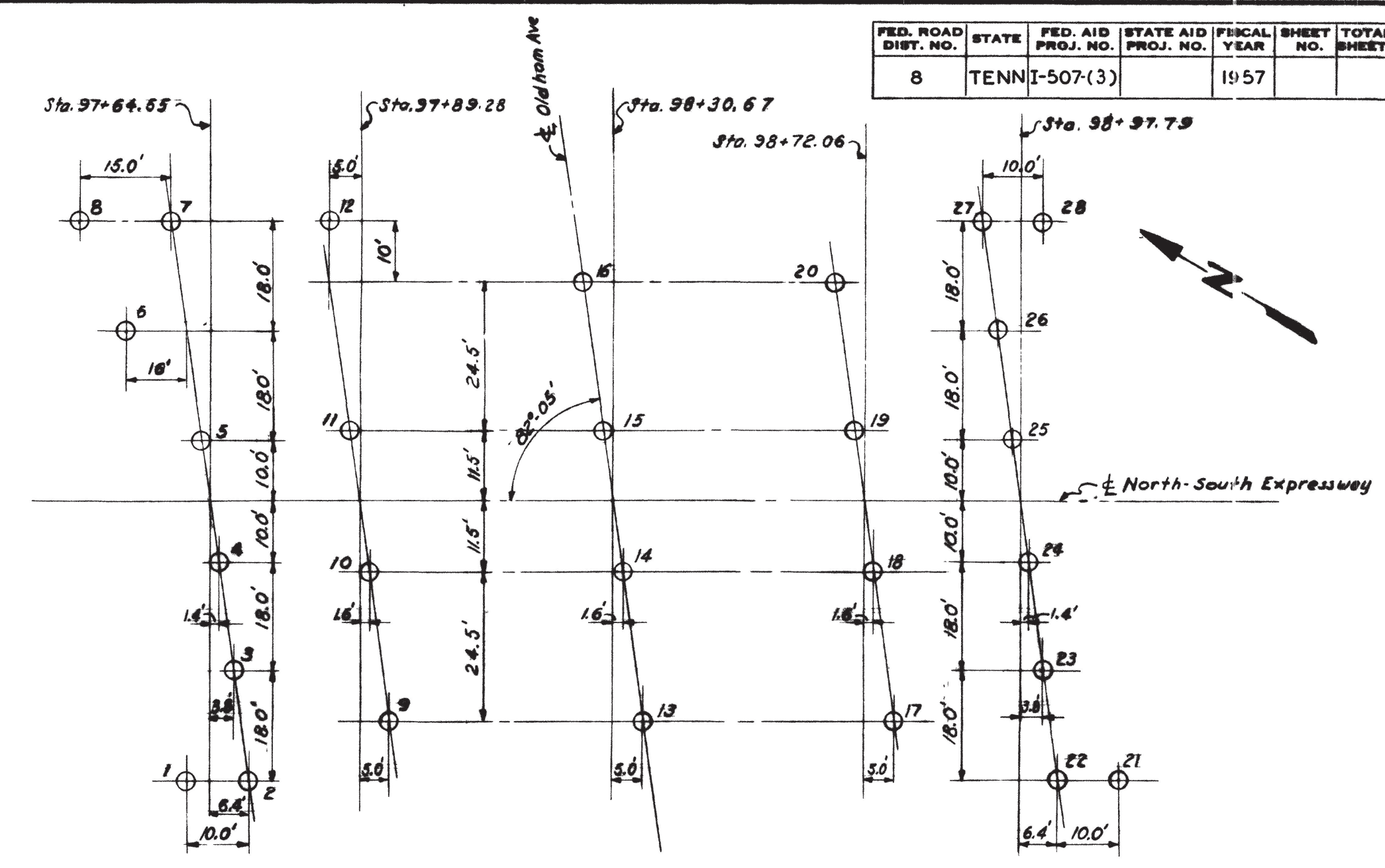
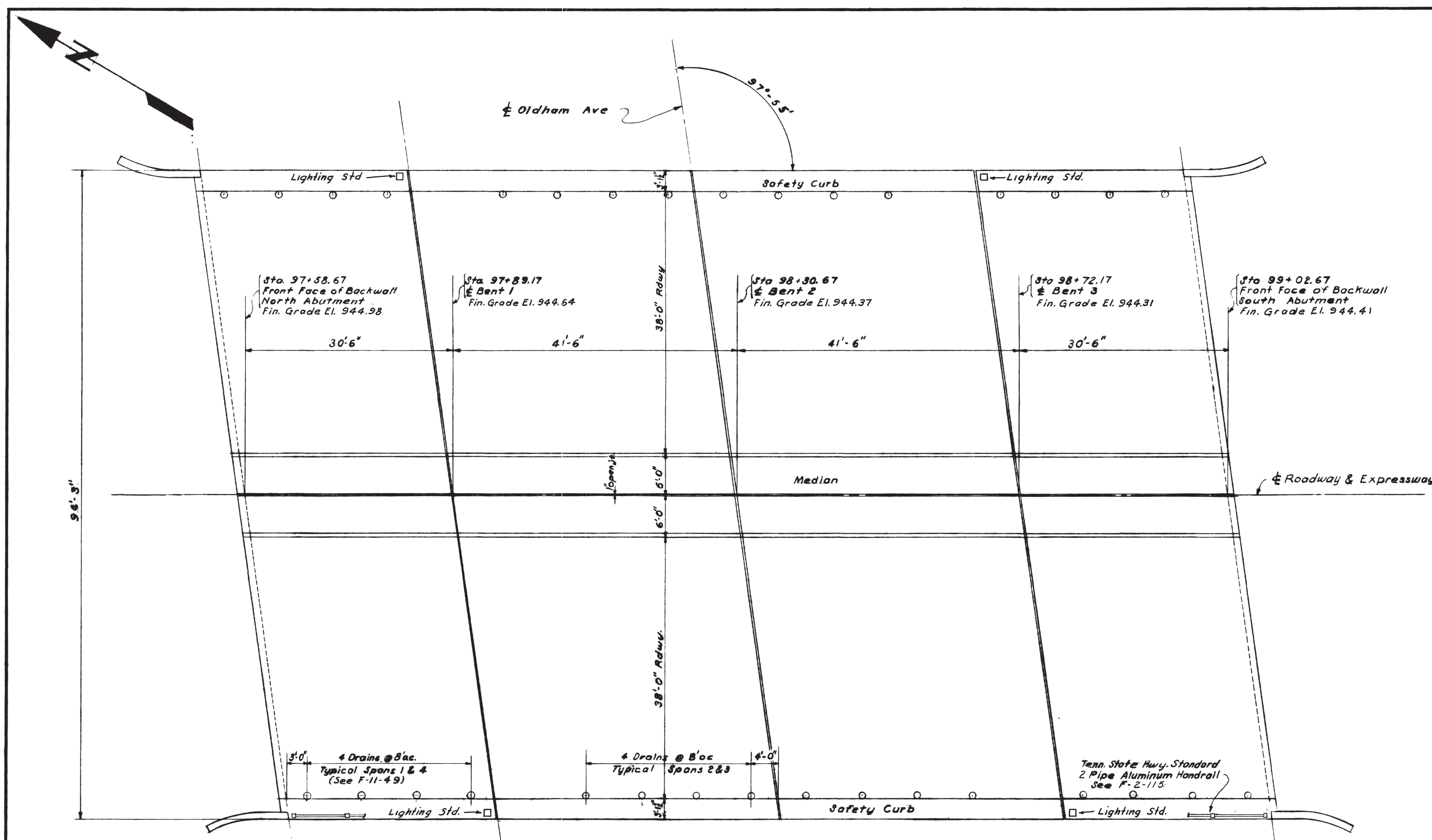
**PHASE II
TMS LAYOUT**

I-275
STA. 347+50 TO
STA. 367+50

10/30/2002
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FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	STATE AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
8	TENNI-507(3)			1957		



SOUNDING PLAN
No Scale

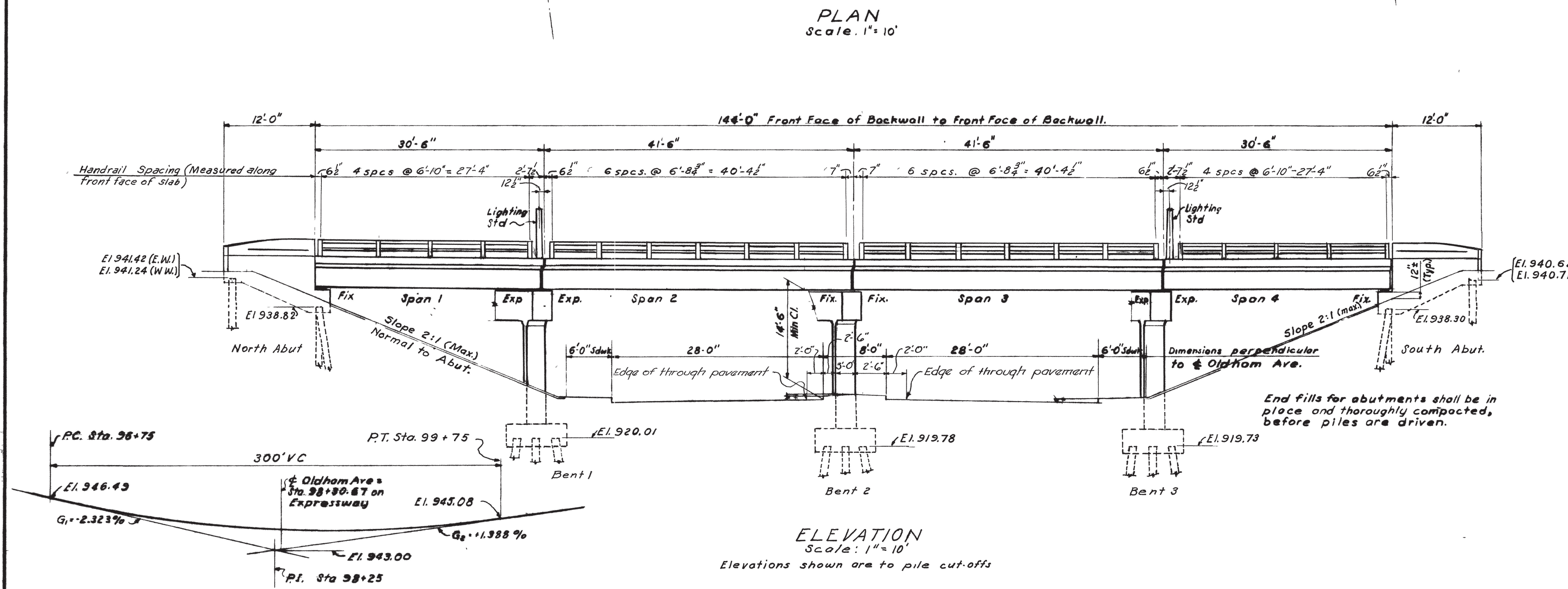
LOCATION	SOUNDING DATA																											
	NORTH ABUT.								BENT 1				BENT 2				BENT 3				SOUTH ABUT.							
HOLE NO.	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28
GROUND EL.	*	*	*	*	927.4	928.0	*	929.2	*	925.6	925.3	925.9	925.3	924.0	924.1	*	925.2	925.8	925.6	931.7	928.1	928.3	928.8	927.9	926.1	*	*	
ROCK EL.	*	*	*	*	900.0	902.0	*	903.0	*	896.6	898.0	898.3	898.1	902.4	*	898.2	902.6	897.6	*	903.1	*	900.3	902.9	896.1	*	*		

* Holes not drilled (Inaccessible)

ESTIMATED QUANTITIES

ITEM	ESTIMATED QUANTITIES									
	NO.17-2	NO.17-4	NO.17-5	NO.132-1	NO.135-4	NO.135-12	NO.133-1	NO.133-3	No.502	No.503
STRUCTURE	DRY	ROCK	ROCK	STRUCT	CLASS A	REINF.	PRECAST C. PILING	HAND-LIGHTING		
	EXC. *	EXC.	DRILLING	STEEL**	CONC.®	STEEL	TEST	SIZE-1	RAIL	COMPLETE
	C.Y.	C.Y.	L.F.	LBS.	C.Y.	LBS.	L.F.	L.F.	L.F.	L.S.®
N. ABUT.	50				55.7	6802	80	560		
BENT 1	110				107.2	16879		900		
BENT 2	110				105.4	16661		792		
BENT 3	110				106.8	16879		864		
S. ABUT.	50				55.7	6802	80	560		
SPAN 1				4,312	127.7	31,870				61
SPAN 2				4,423	180.0	48,275				83
SPAN 3				4,423	180.0	48,275				83
SPAN 4				4,312	127.7	31,870				61
TOTAL	430	0	0	17,470	1046.2	224,313	160	3676	288	L.S.

* All earth excavation shall be measured and paid for as dry excavation only.
 ** Includes bronze alloy plates in bearings.
 ® Cost of all embedded material, such as joint filler, drains, sheet piling, etc., shall be in unit price of Class "A" concrete.
 ® Lump Sum for furnishing and placing all conduits, wire, junction boxes, lighting standards, recessed light fixtures and all other accessories necessary to illuminate the structure.



ELEVATION
Scale: 1"=10'

Elevations shown are to pile cut-offs

Notes:
 For General Notes and Specifications, see F-11-B.
 For Electrical Lighting Details, see F-11-10 and F-11-100.
 For Handrailing Details, see F-2-115 & F-11-49.
 All handrail posts and electrical lighting stds. shall be vertical.
 Two 38'-0" Roadways with two Safety Curbs and 12'-0" Median.

LIST OF DRAWINGS

NO	TITLE
F-11-45	General Drawing
F-11-46	Anchor Bolt Plan
F-11-47	Bent Details
F-11-48	Beam Details
F-11-49	Deck Details
F-11-50	Reinforcing Steel

For Abutment Details, see F-11-13, F-11-15, & F-11-28 (Heiskell Avenue Overpass)

F-11-100 Electrical Lighting Details

STATE OF TENNESSEE
 DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS
 NORTH-SOUTH EXPRESSWAY
 KNOX COUNTY - F.A. PROJ. NO. I-507 - (3)

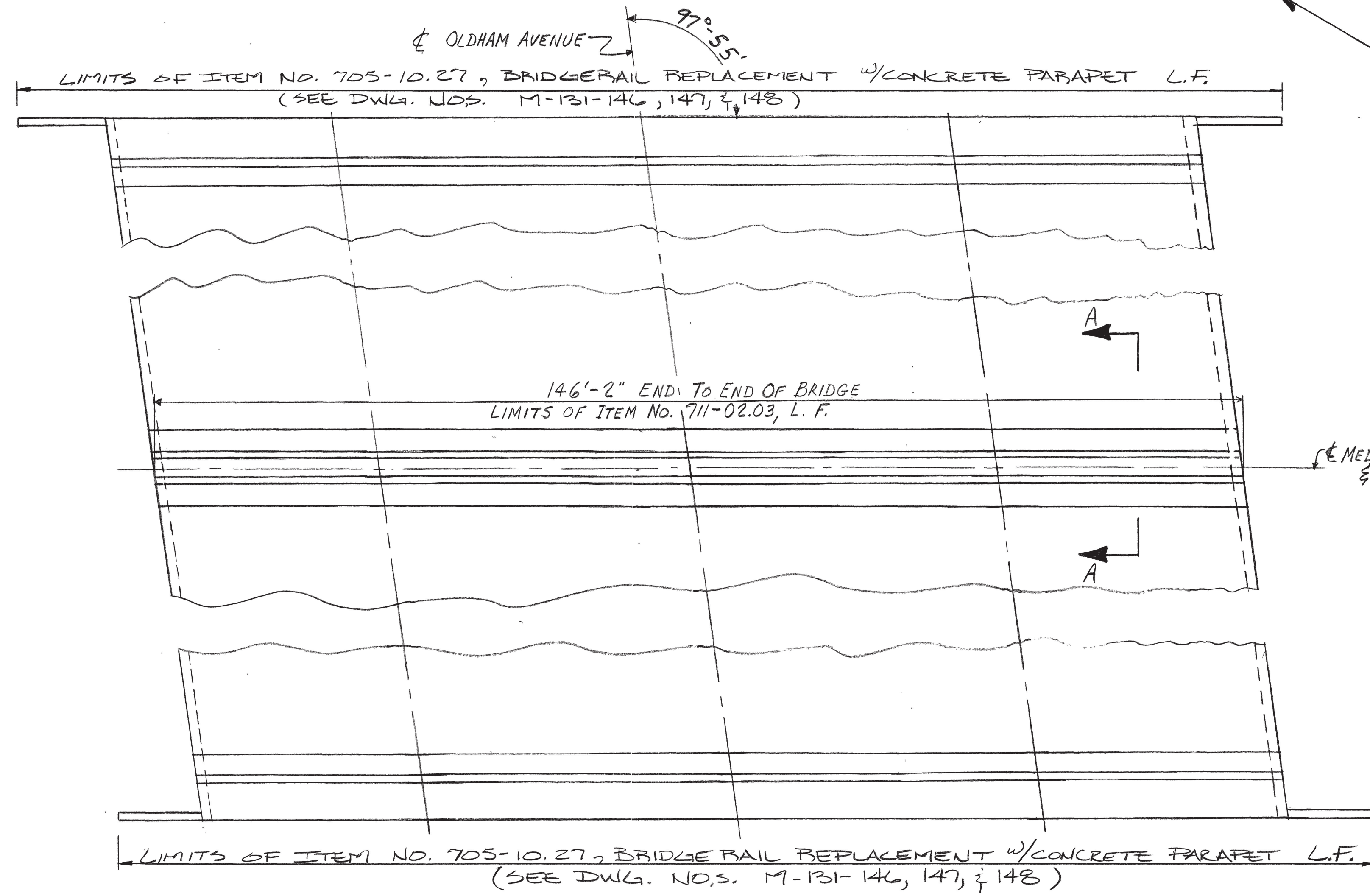
OLDHAM AVENUE UNDERPASS
 GENERAL DRAWING

DSGN: TMS	AAKE F. HEDMAN	JUNE 24, 1957
DRWN: TMS	CONSULTING ENGINEER	
CHKD: AC	CHATTANOOGA, TENNESSEE	
SUPV: AFH	SCALE: AS NOTED FILE NO.: 56.70	F-11-45

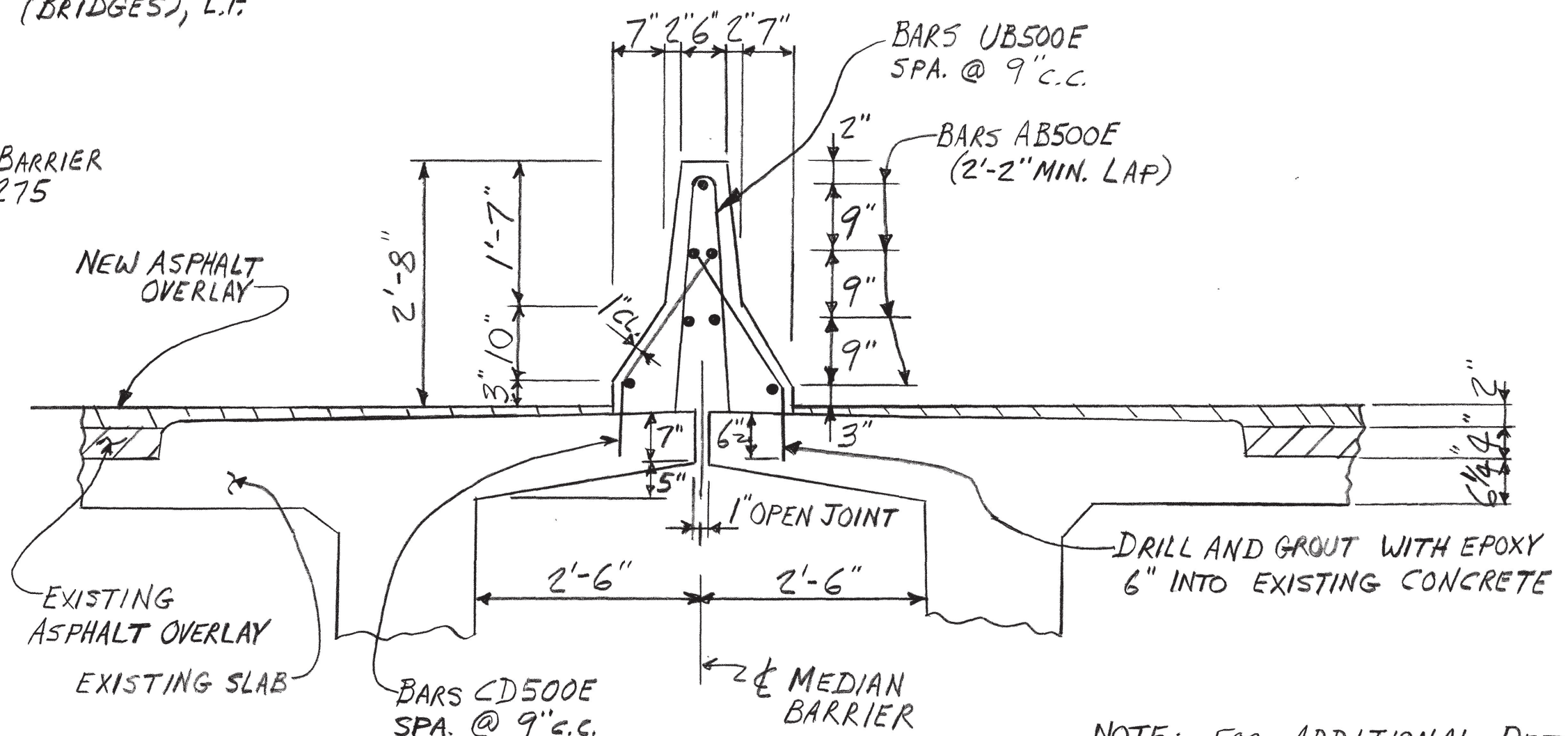
Revised: 10-28-57 Clearances added

PROJECT NO.	YEAR	SHEET NO.
BR-275-362	1988	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	6-16-88	T.C.	THIS SHEET ADDED TO PLANS.
2	6-30-88	WRW	CHANGED COLOR OF TEXTURE COATING.



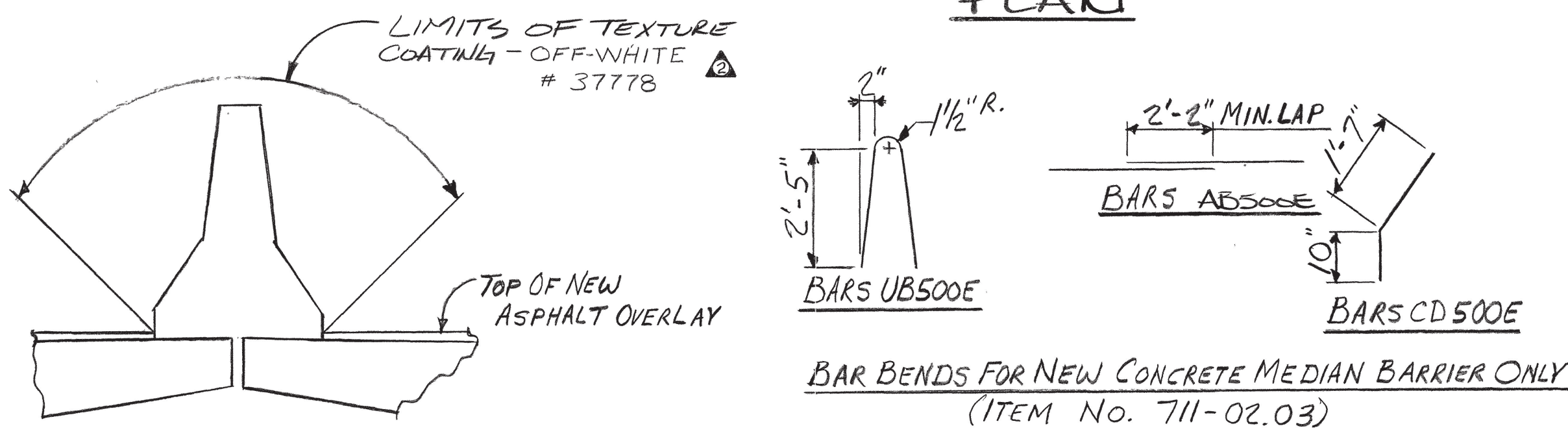
NOTE: COST OF ALL CLASS "A", $f'_c = 3,000$ PSI. CONCRETE, EPOXY COATED REINFORCING STEEL, FORMING, TEXTURE COATING, LABOR, AND MISCELLANEOUS MATERIALS NEEDED TO CONSTRUCT THE CONCRETE MEDIAN BARRIER AS SHOWN IN THESE DETAILS TO BE INCLUDED UNDER ITEM NO. 711-02.03, REINFORCED CONCRETE MEDIAN BARRIER (BRIDGES), L.F.



SECTION A-A
(SHOWING CONCRETE MEDIAN BARRIER)

NOTE: FOR ADDITIONAL DETAILS AND NOTES FOR THE CONCRETE MEDIAN BARRIER, SEE STANDARD DRAWING NO. M-28-1A.

PLAN



TEXTURE COATING DETAIL
(COST OF TEXTURE COATING TO BE INCLUDED UNDER ITEM NO. 711-02.03, L.F.)

BAR BENDS FOR NEW CONCRETE MEDIAN BARRIER ONLY
(ITEM NO. 711-02.03)

NOTE: COST OF REMOVING EXISTING MEDIAN GUARDRAIL TO BE INCLUDED IN ITEM NO. 711-02.03, REINFORCED CONCRETE MEDIAN BARRIER (BRIDGES), L.F.

NOTE: 1" OPEN JOINT TO BE FILLED WITH BITUMINOUS MATERIAL BEFORE POURING NEW CONCRETE MEDIAN BARRIER. COST TO BE INCLUDED IN ITEMS BID ON.

NOTE: BRIDGE DECK DRAINS TO BE CLEANED AND MAINTAINED OPEN DURING CONSTRUCTION OF NEW PARAPET. COST TO BE INCLUDED IN ITEMS BID ON.

NOTE: FOR DETAIL AND NOTES ON BRIDGE RAIL REPLACEMENT W/ CONCRETE PARAPET (ITEM NO. 705-10.27). REFER TO DWG. NO. M-131-146, M-131-147, & M-131-148.

ESTIMATED QUANTITY

711-02.03 REINFORCED MEDIAN BARRIER (BRIDGES)	* 705-10.27 BRIDGE RAIL REPLACEMENT W/ CONCRETE PARAPET (DETAIL GRA-27)
L.F.	L.F.
147	240

* FOR ITEM NO. 705-10.27, SEE ROADWAY SHEET 2A.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

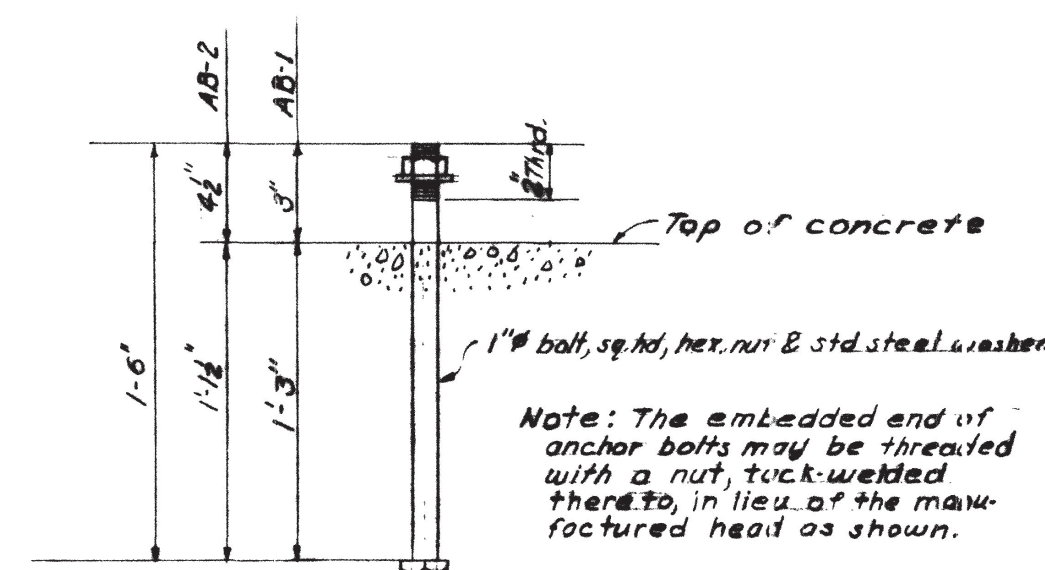
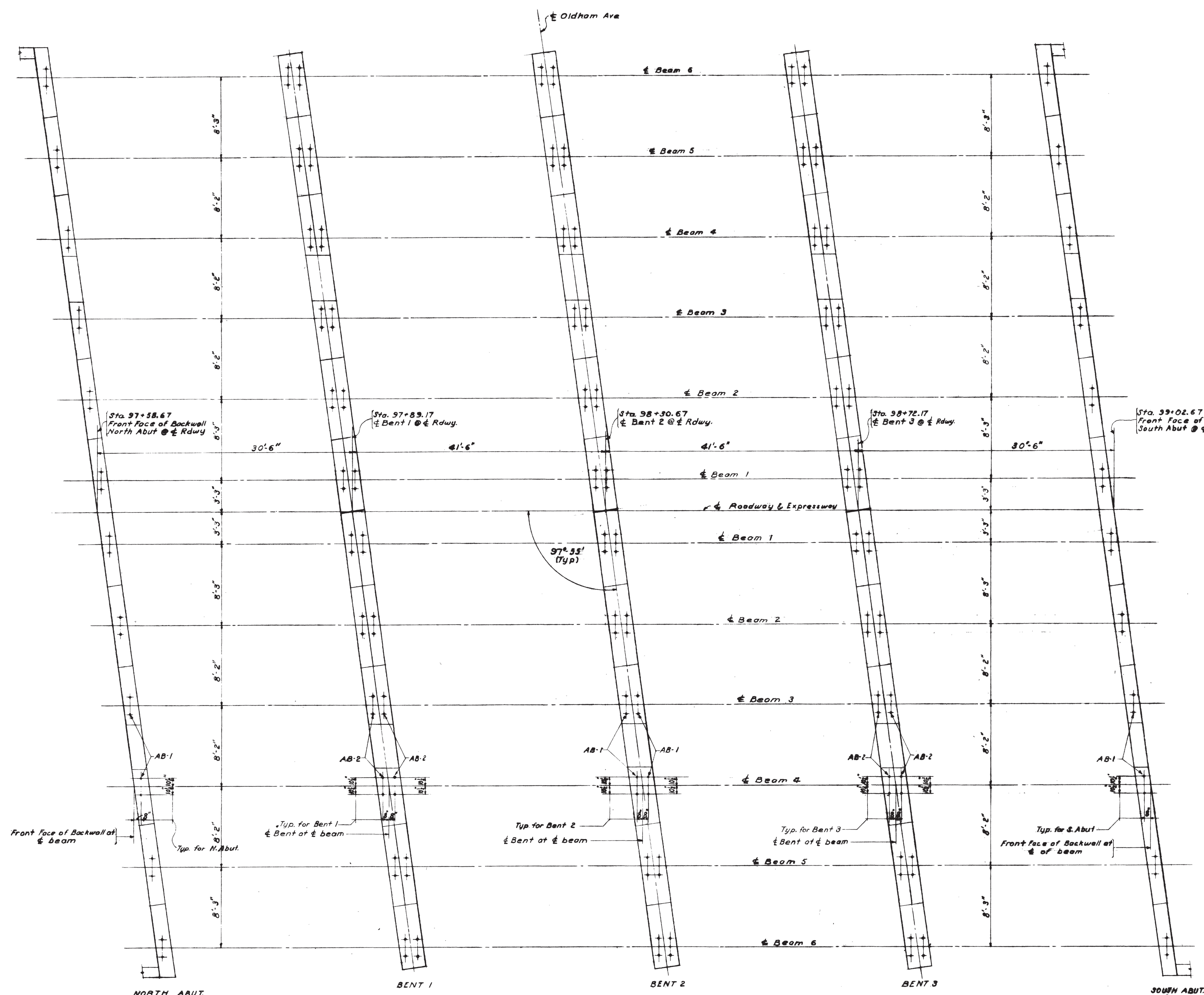
BRIDGE REPAIR DETAILS
I275 OVER OLDHAM AVENUE
BRIDGE NO. 47-I275-1.02
KNOX COUNTY
1988

DESIGNED BY T. QUINN DATE 3-88
DRAWN BY G. ISBELL DATE 3-88
SUPERVISED BY GENTRY AND MCINTURE DATE 3-88
CHECKED BY GRAVES AND HICKS DATE 3-88

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES
APPROVED Lewis Evans
DIRECTOR OF HIGHWAYS

F-11-45A

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	STATE AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
8	TENN	I-507(3)		1957		



DETAIL OF ANCHOR BOLTS
192 Req'd

Note: Bolts marked AB-1 project 3" & are for fixed shoes. Bolts marked AB-2 project 4" & are for exp. shoes.

All exposed parts of bolt assembly shall be coated with a heavy grease after concrete is placed.

Notes:
For General Notes & Specifications, see F-11-8.
For details of bearings for beams, see F-11-9.

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS
NORTH-SOUTH EXPRESSWAY
KNOX COUNTY - F.A. PROJ. NO. I-507 - (3)

OLDHAM AVENUE UNDERPASS
ANCHOR BOLT PLAN

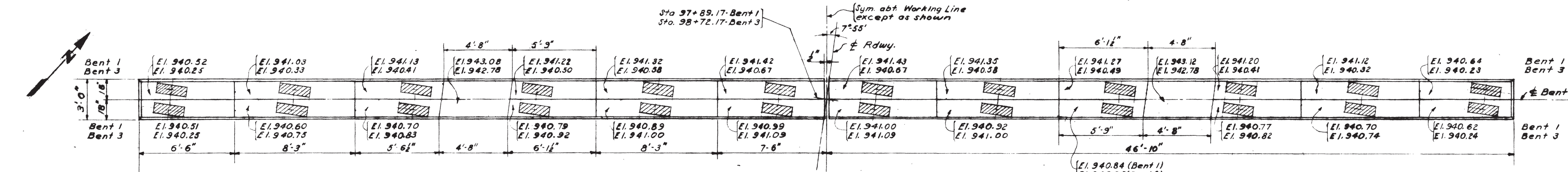
DSGN: TMS DRWN: TMS CHKD: AC SUPV: AFH	AAKE F. HEDMAN CONSULTING ENGINEER CHATTANOOGA, TENNESSEE	JUNE 24, 1957
SCALE: NONE	FILE NO.: 56.70	F-11-46

MICROFILMED

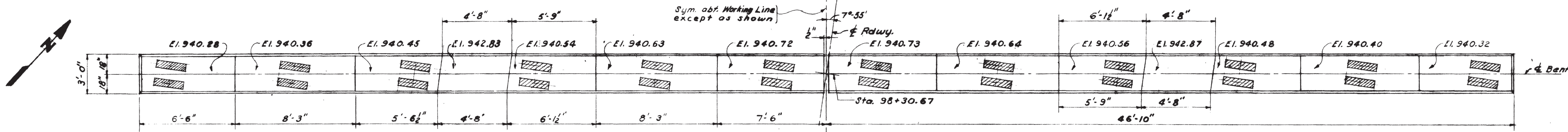
ANCHOR BOLT PLAN

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	STATE AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
8	TENNESSEE			1957	19	26

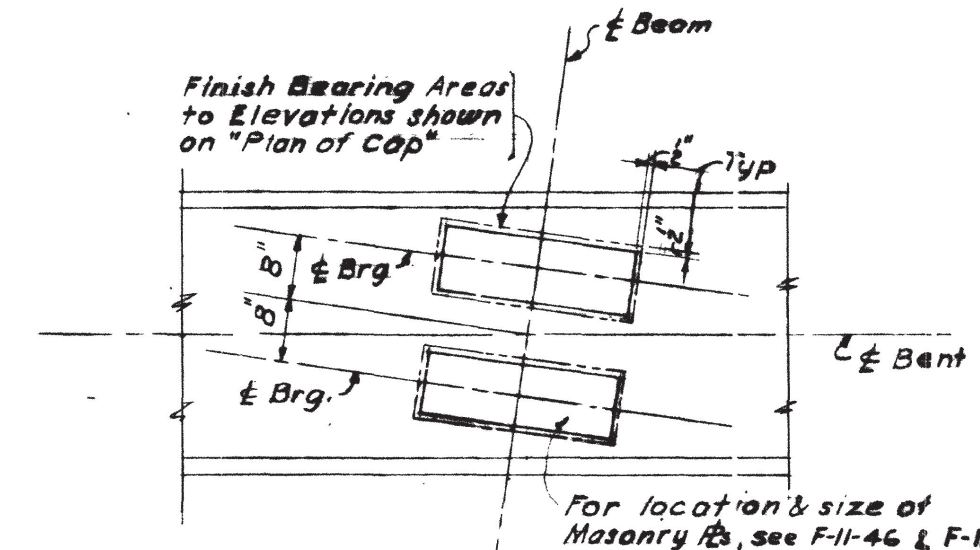
I-75-3(19)85



PLAN OF CAP-BENTS 1&3

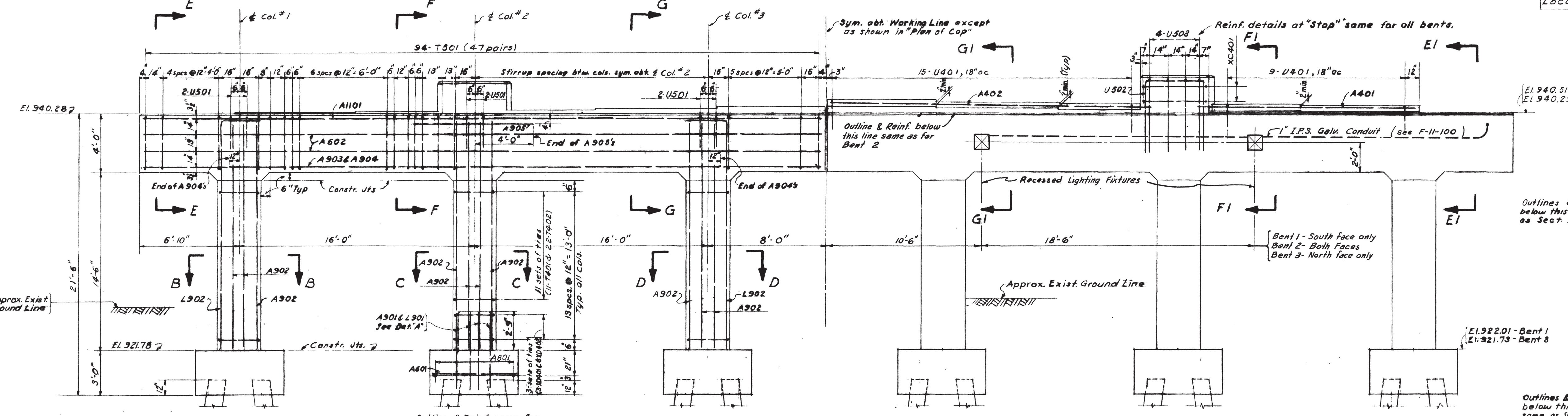


PLAN OF CAP - BENT 2



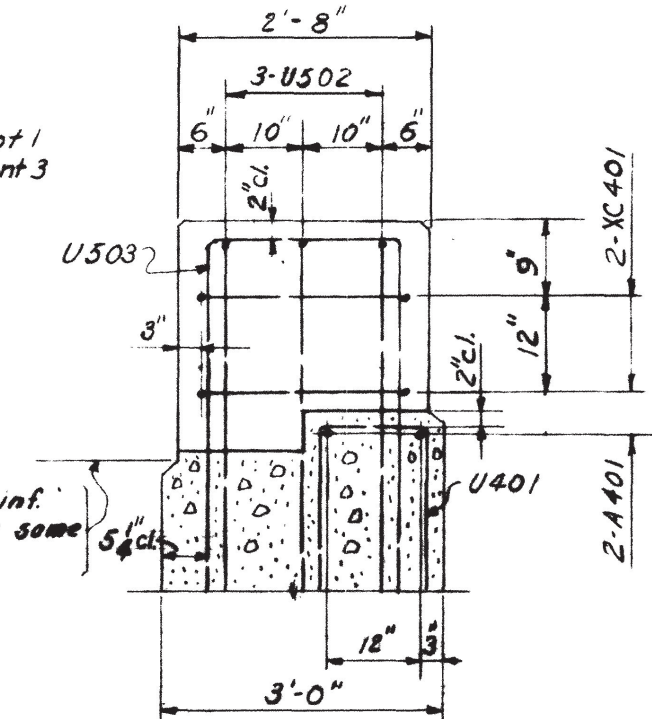
DETAIL OF BEARING AREA Scale: 1/2"=1'-0"

When pouring caps, provision shall be made for setting anchor bolts for bearing plates. Location of bolts is given on F-11-46.

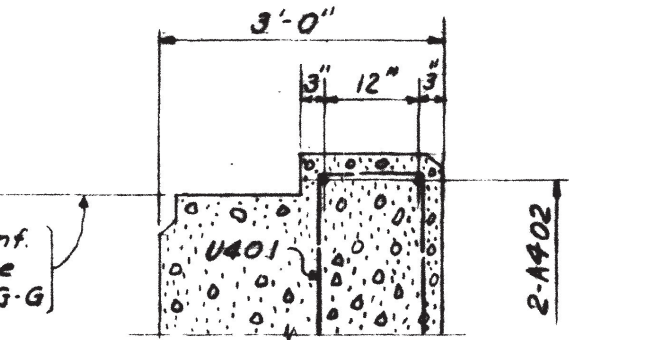


HALF ELEVATION - BENT 2

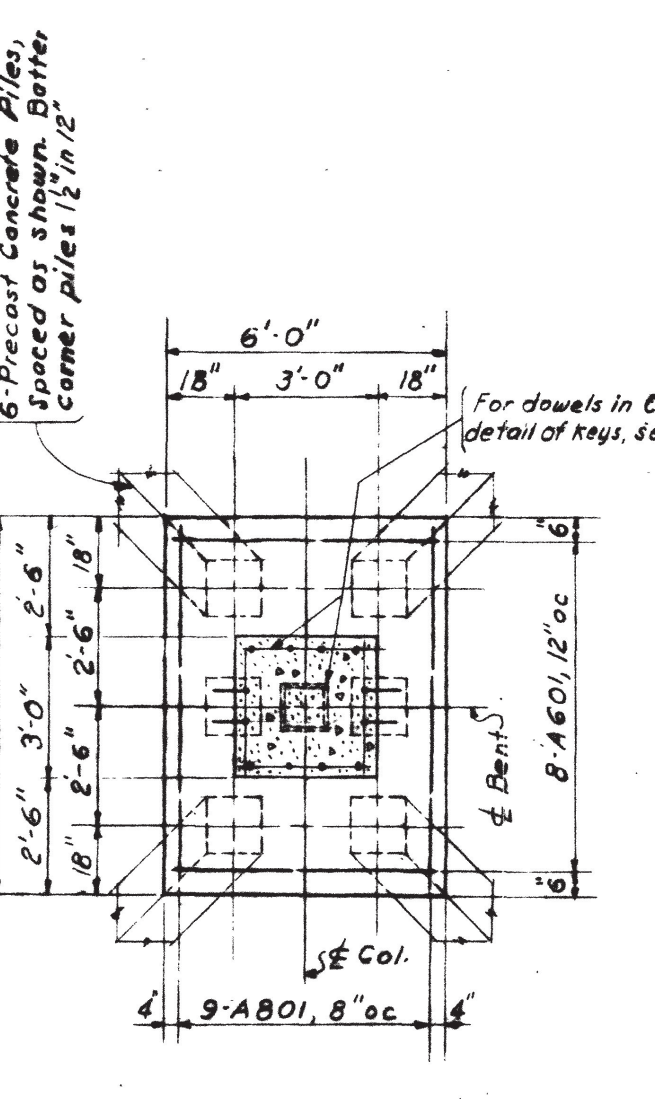
HALF ELEVATION - BENTS 1&3 BENT 1 SHOWN



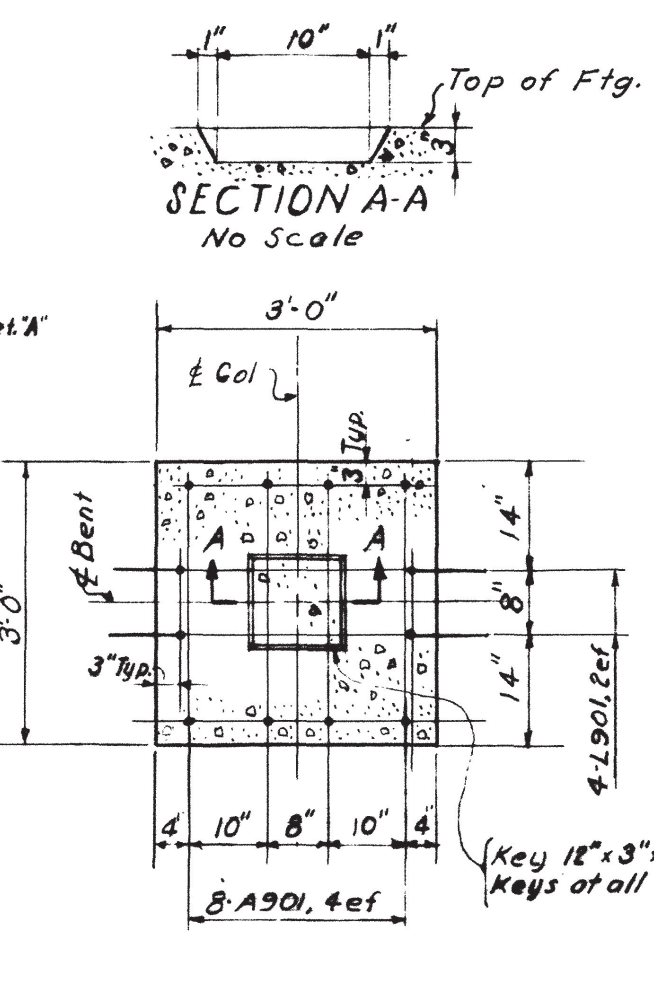
PART-SECTION F1-F1 Scale: 1/2"=1'-0"



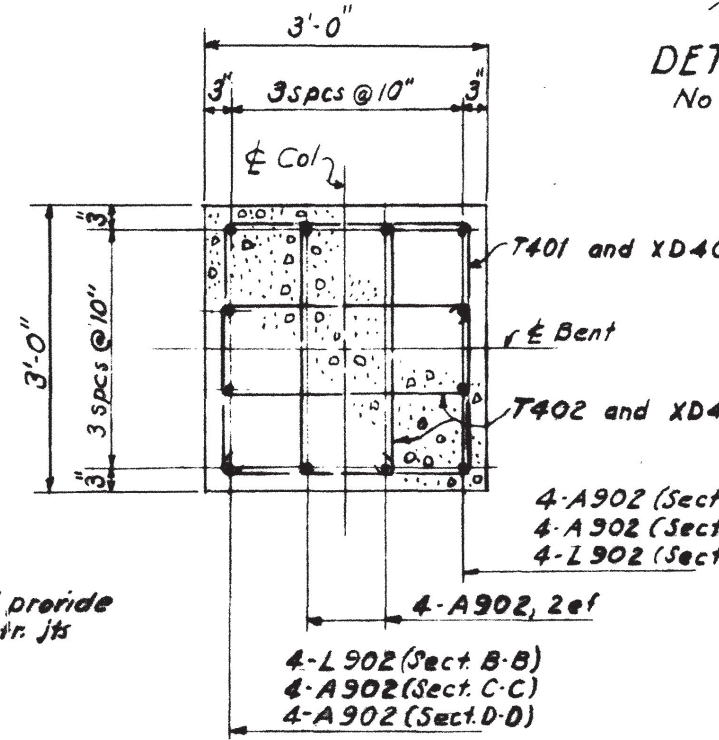
PART-SECTION G1-G1 Scale: 1/2"=1'-0"



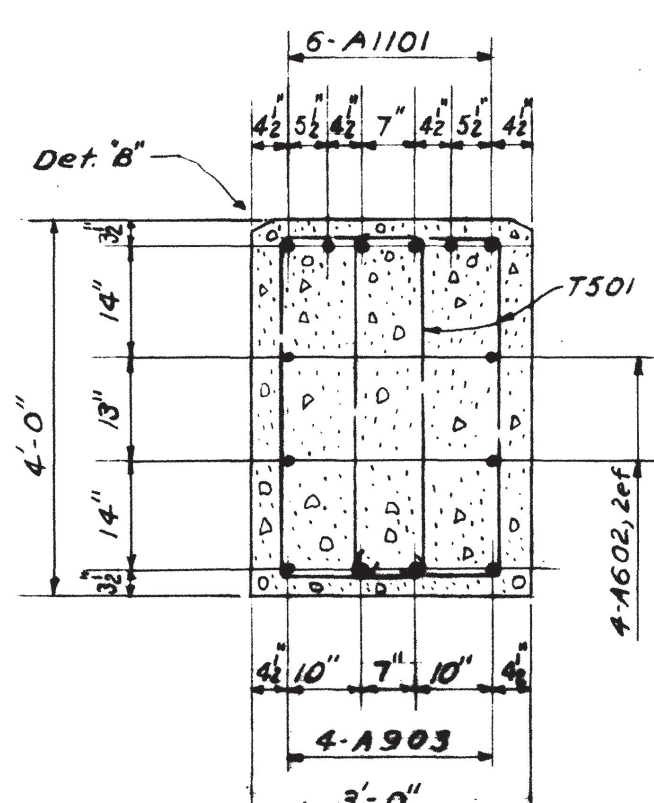
PLAN OF FOOTING



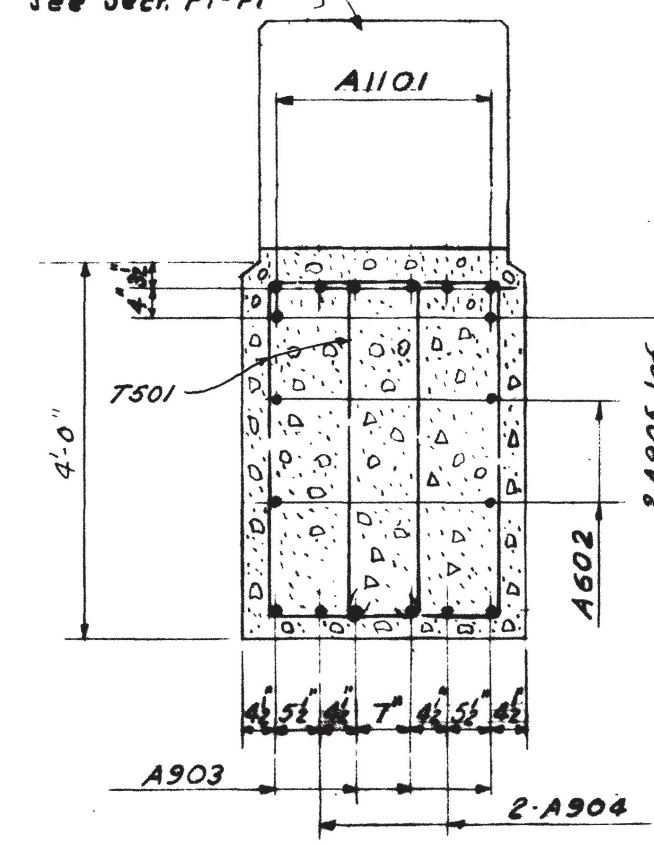
SECTION A-A Scale: 1/2"=1'-0"



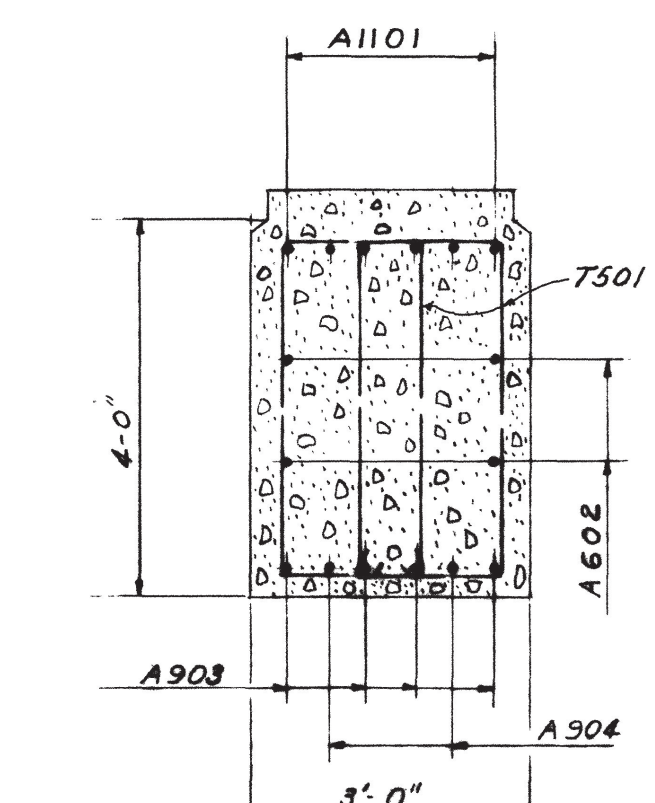
SECTIONS B-B, C-C, & D-D Scale: 1/2"=1'-0"



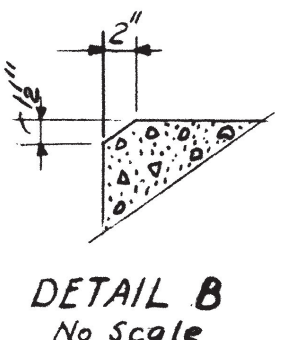
SECTION E-E SECT. E1-E1 SIMILAR Scale: 1/2"=1'-0"



SECTION F-F Scale: 1/2"=1'-0"



SECTION G-G Scale: 1/2"=1'-0"



DETAIL B No Scale

Abbreviations ef - each face

Notes:
 For General Notes & Specifications, see F-11-8.
 For location of bents, see F-11-45.
 For details of piles, see F-2-118.
 For Electrical Lighting Details, see F-11-10 and F-11-100.
 For reinforcing steel and bending diagrams, see F-11-50.
 Chamfer all exposed edges 1/4", except as noted.
 All dimensions relative to spacing of reinforcing steel are to centers of bars, except as noted.
 Marks to all reinforcing steel in bents shall have suffix B. (Thus A601-B, U501-B, etc.)
 Piles shall be driven to refusal on rock or to obtain a minimum bearing capacity of 30 tons per pile.

STATE OF TENNESSEE
 DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS
 NORTH-SOUTH EXPRESSWAY
 KNOX COUNTY - F.A. PROJ. NO. I-507- (3)

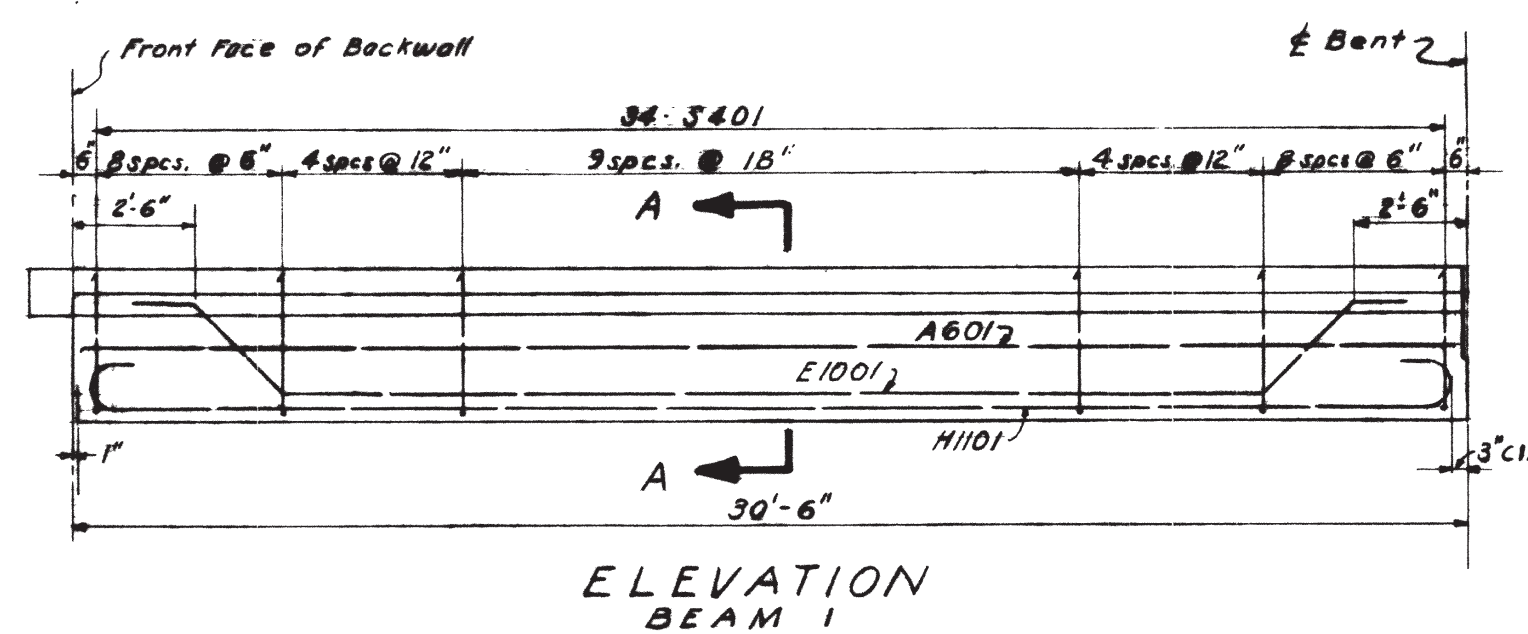
OLDHAM AVENUE UNDERPASS
 BENT DETAILS

DSGN: TMS	AAKE F. HEDMAN	JUNE 24, 1957
DRWN: TMS	CONSULTING ENGINEER	
CHKD: AC	CHATTANOOGA, TENNESSEE	
SUPV: AFH	SCALE: 1/4"=1'-0" UNLESS NOTED	FILE NO.: 56.70

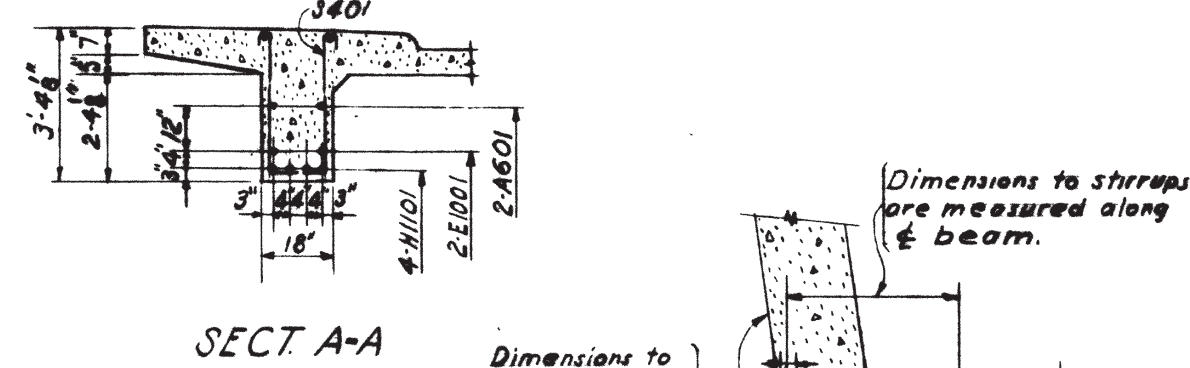
F-11-47

MICROFILMED

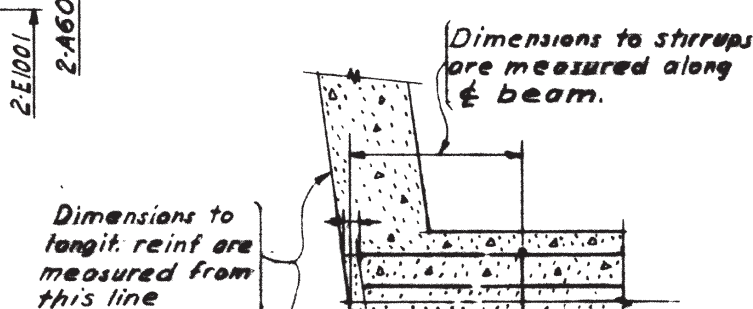
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	STATE AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
8	TENN	I-507(3)		1957		



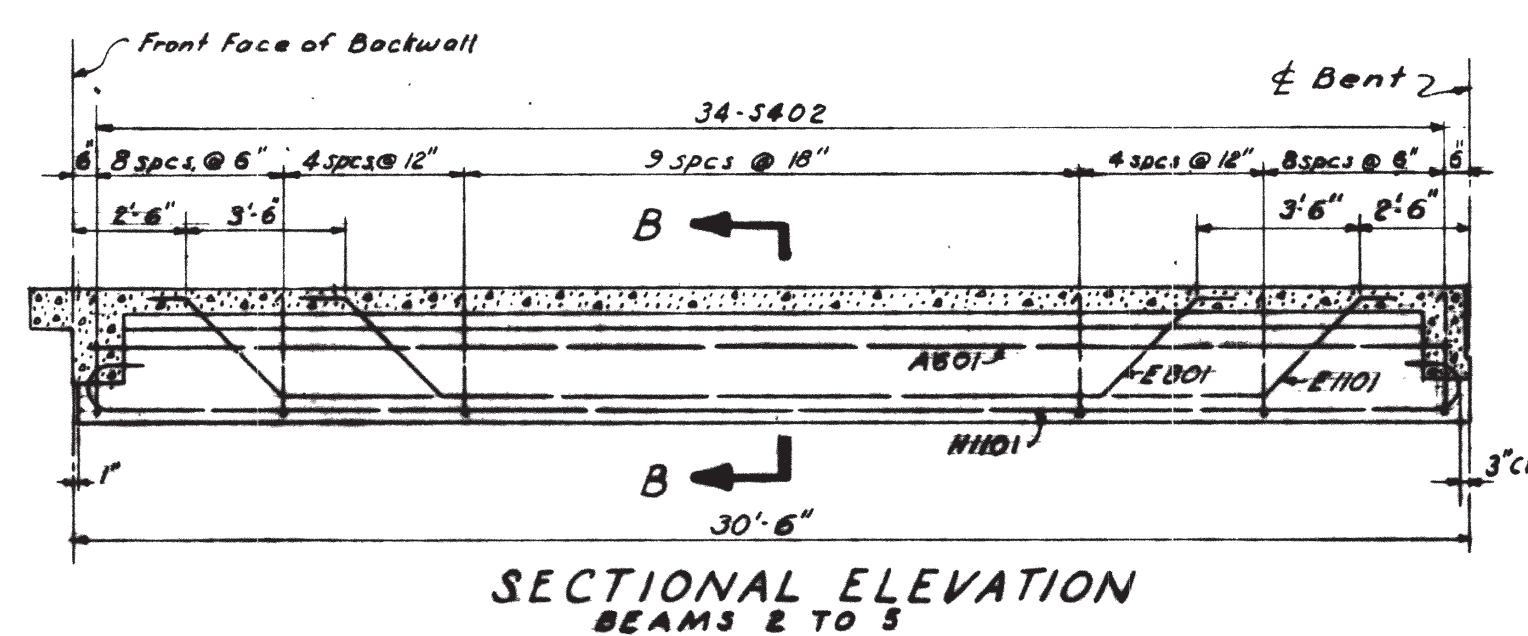
ELEVATION BEAM 1



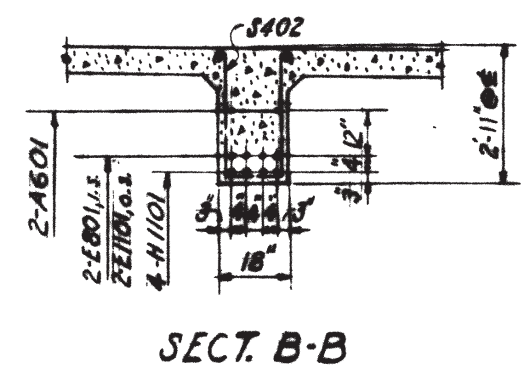
SECT A-A



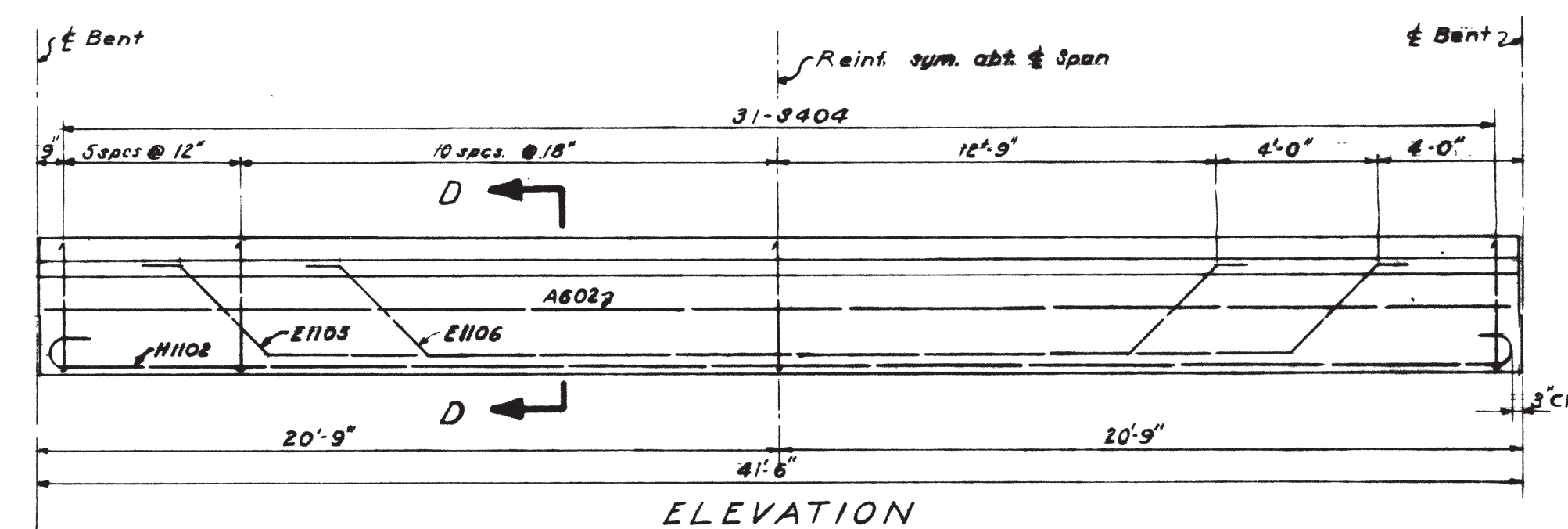
DETAIL "A" No Scale



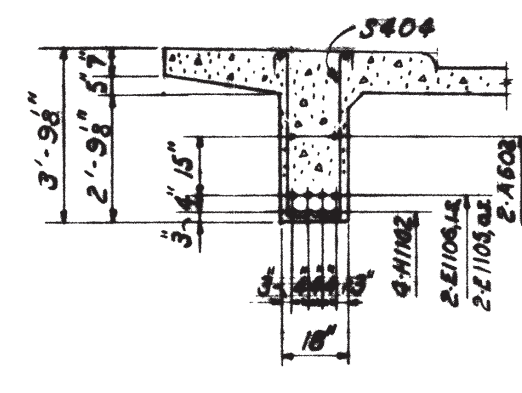
SECTIONAL ELEVATION BEAMS 2 TO 5



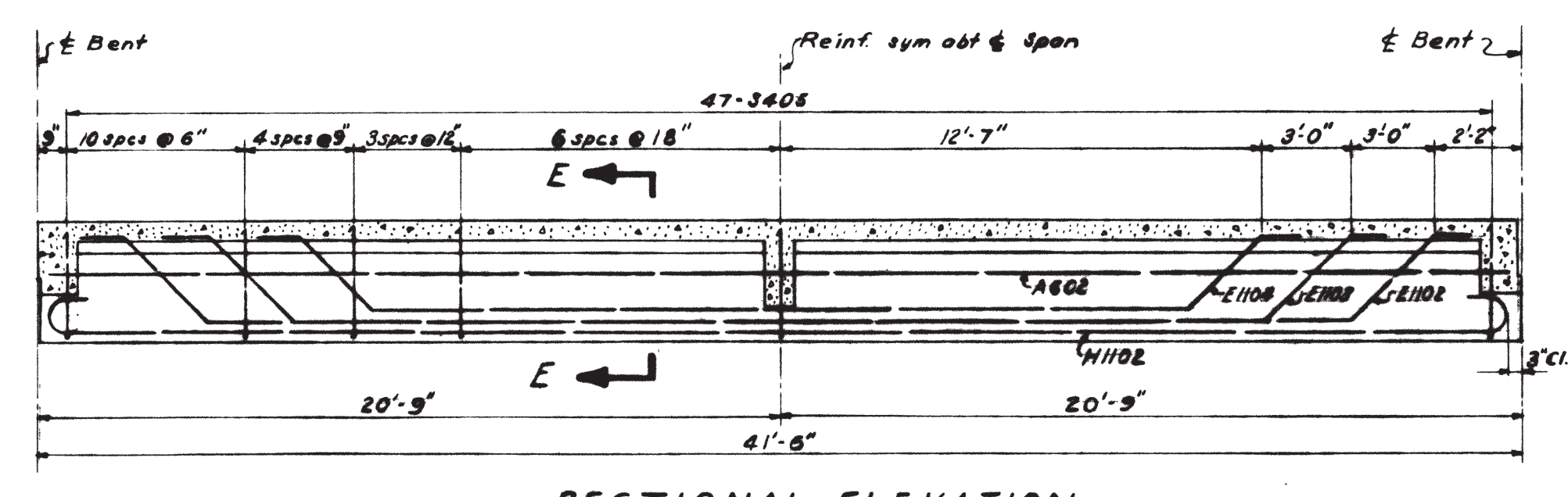
SECT B-B



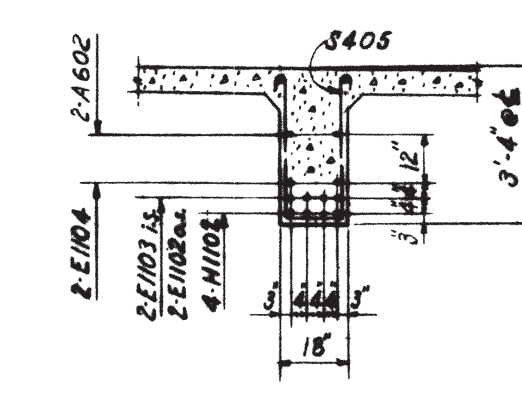
ELEVATION BEAM 1



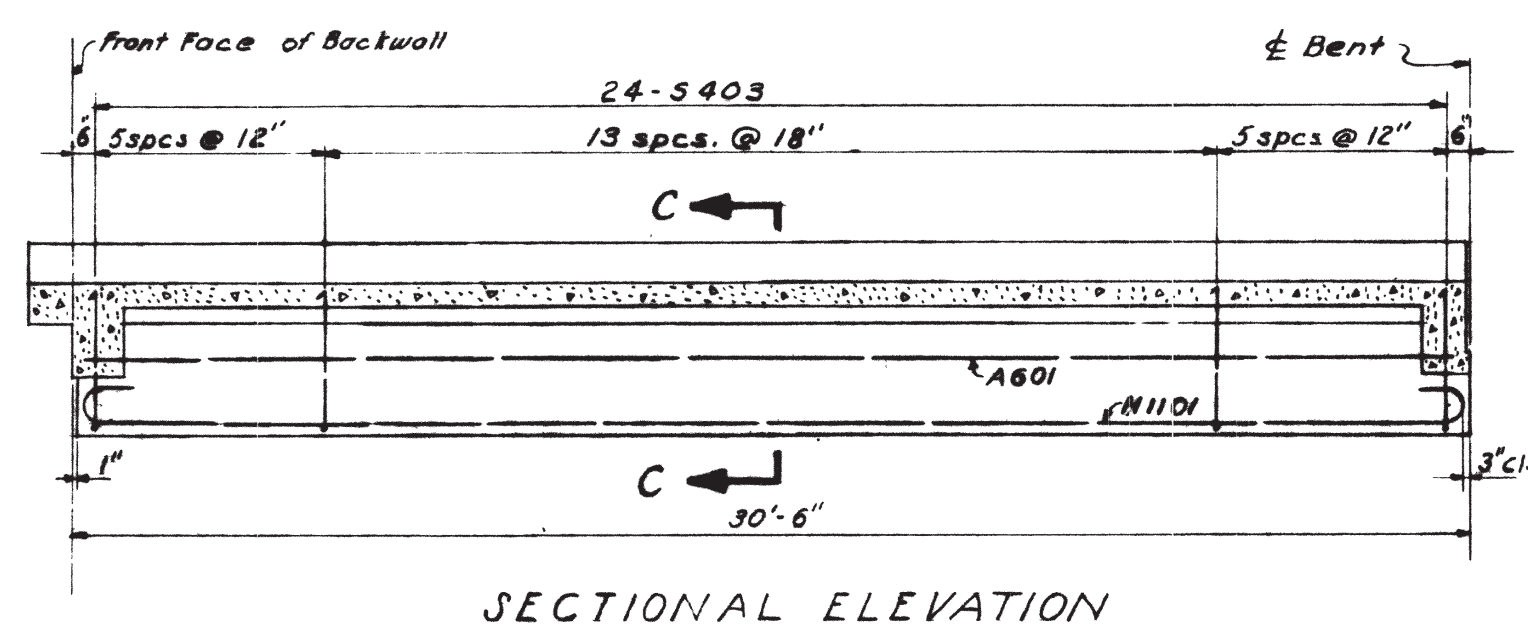
SECT D-D



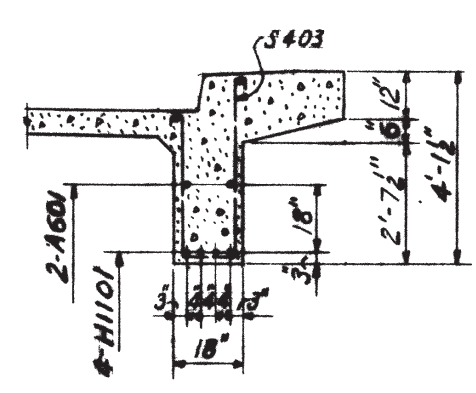
SECTIONAL ELEVATION BEAMS 2 TO 5



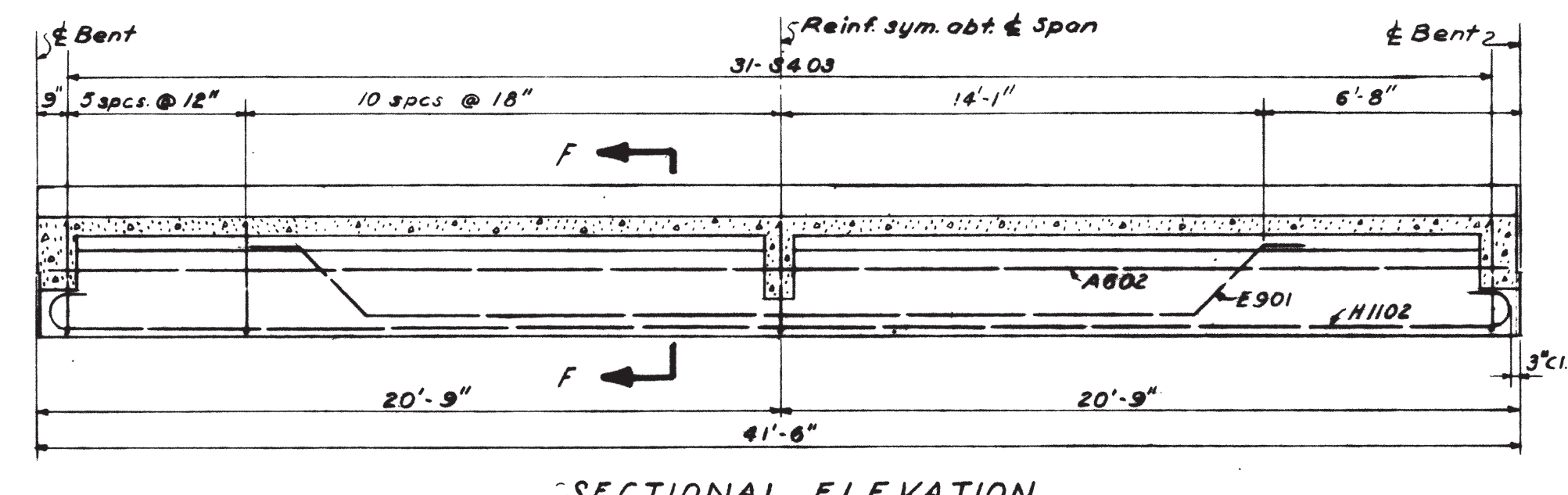
SECT E-E



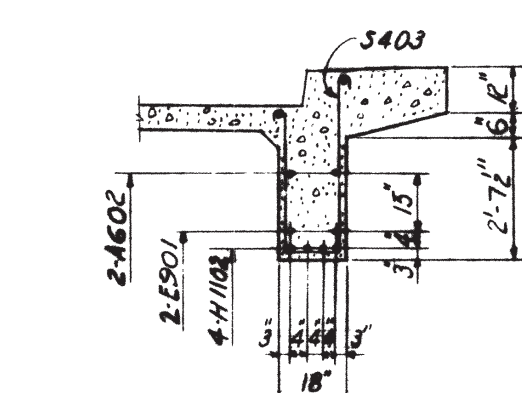
SECTIONAL ELEVATION BEAM 6



SECT C-C



SECTIONAL ELEVATION BEAM 6

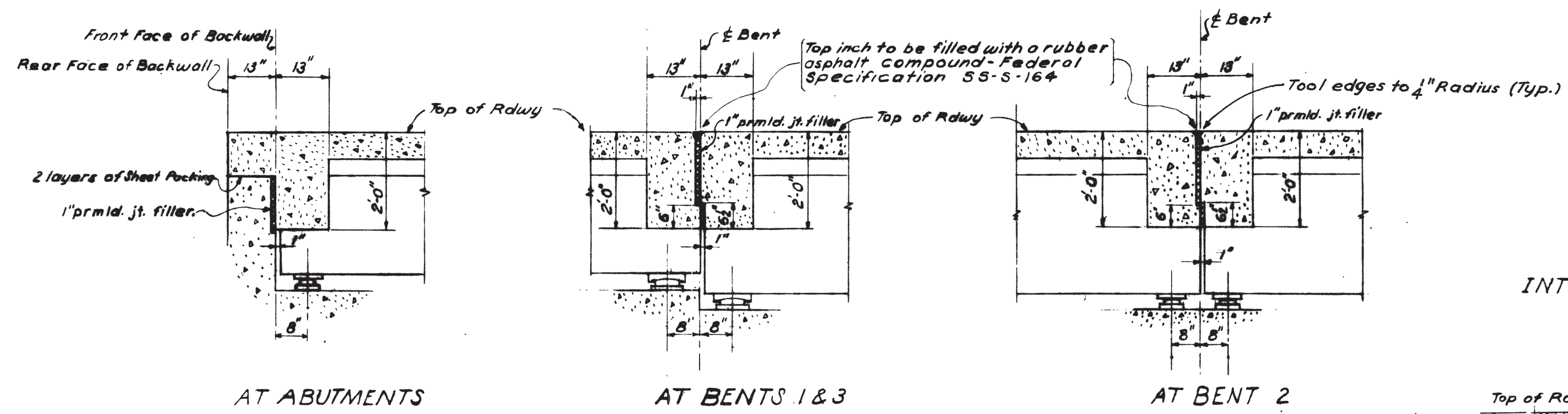


SECT F-F

Dimensions for longitudinal reinforcement are measured parallel to ϵ of beam. See Detail "A".

BEAM DETAILS - SPANS 1 & 4

BEAM DETAILS - SPANS 2 & 3

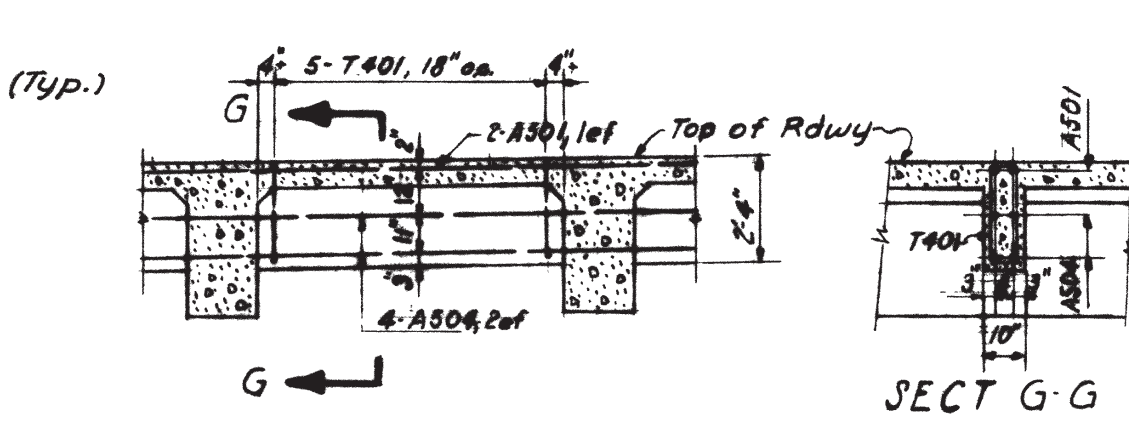


AT ABUTMENTS

AT BENTS 1 & 3

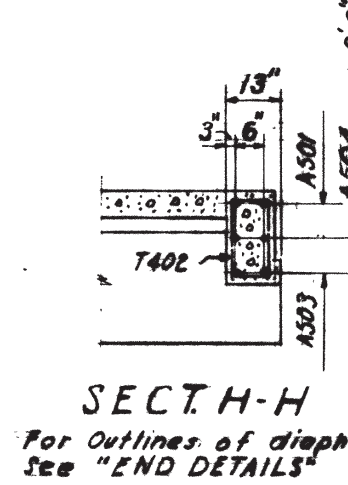
AT BENT 2

END DETAILS Scale: 1/2"=1'-0"

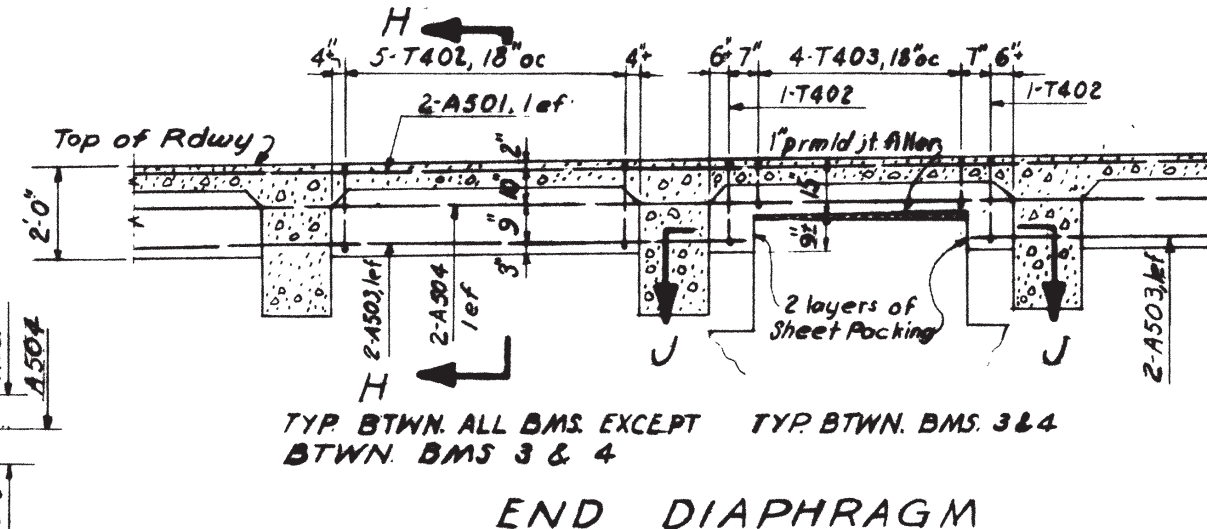


INTERMEDIATE DIAPH. SPANS 2 & 3 TYP. BTWN. BMS.

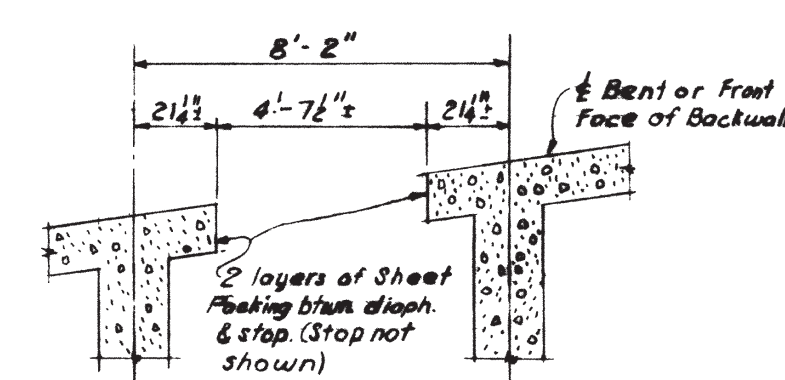
SECT G-G



SECT H-H For Outlines of diaphragms. See "END DETAILS"



END DIAPHRAGM DIAPHRAGM DETAILS



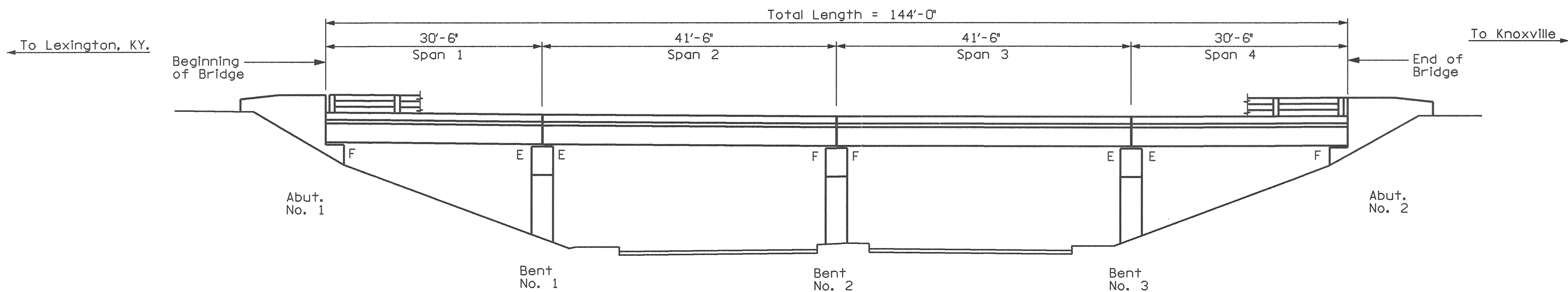
SECT. PLAN U-U

Abbreviations is. inside steel o.s. outside steel ef. each face

Notes: For General Notes and Specifications, see F-11-B. For details of bearings for beams, see F-11-9. For reinforcing steel and bending diagrams, see F-11-50. All dimensions relative to spacing of reinforcing steel are to centers of bars, except as noted. Chamfer all exposed edges 1/4" except as noted. Sheet Packing shall be placed with graphite surfaces in contact. Marks to all reinf. steel in deck shall have suffix "D". (Thus A602-D; E1104-D, etc.)

STATE OF TENNESSEE		
DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS		
NORTH-SOUTH EXPRESSWAY		
KNOX COUNTY - F.A. PROJ. NO. I-507- (3)		
OLDHAM AVENUE UNDERPASS		
BEAM DETAILS		
DSGN: AC-TS	AAKE F. HEDMAN	JUNE 24, 1957
DRWN: TMS	CONSULTING ENGINEER	
CHKD: AC	CHATTANOOGA, TENNESSEE	
SUPV: AFH	SCALE: 1/4"=1'-0" UNLESS NOTED	FILE NO.: 56.70
		F-11-48

E:\TN10567\10567.DGN 03-FEB-1998 14:03 STR

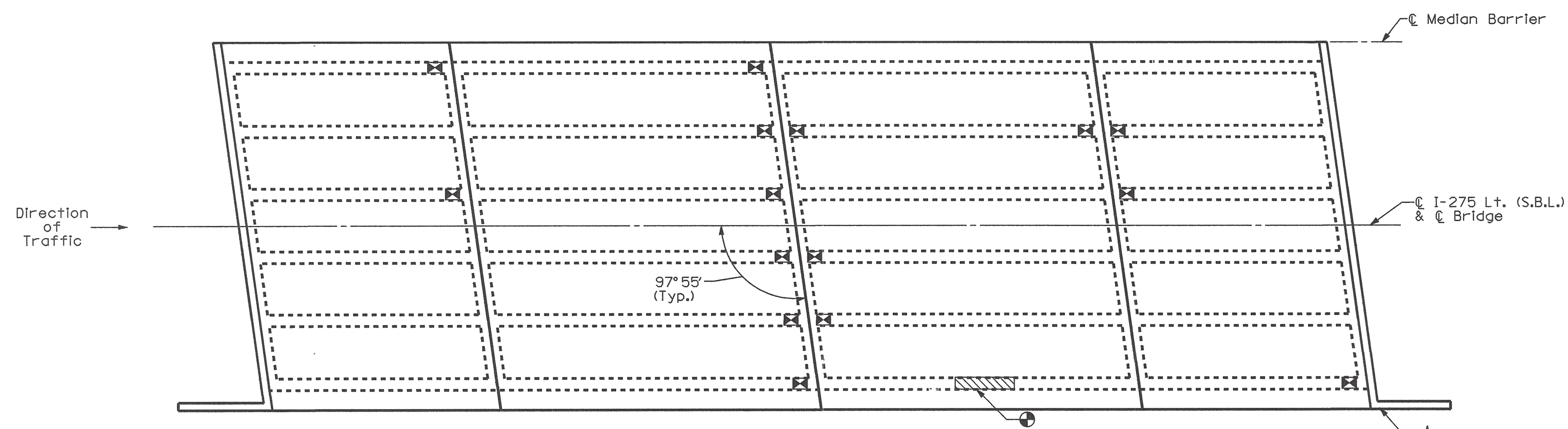


E = Expansion End
F = Fixed End

Posted Speed Limit = 55 mph

PROJECT NO.	YEAR	SHEET NO.	
47005-4153-04	1998		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	1-16-98	RJC	LATEST REV. DATES
2	2-2-98	WSA	LATEST REV. DATES
3	2-26-98	RJC	LATEST REV. DATES

ELEVATION



PLAN

GENERAL SCOPE OF WORK

- 1) Remove existing asphalt overlay from bridge deck and transitions off each end of bridge. See Dwg. No. BR-27-94 for notes and details.
- 2) Place new 3/4" asphalt overlay with deck sealant for full bridge length and width with transitions at bridge ends. See Dwg. No. BR-27-94 for notes and details.
- 3) Clean and seal three (3) existing bituminous joints (Bents 1-3). See Dwg. No. BR-27-95 for notes and details.
- 4) Repair areas of spalled concrete. See Dwg. No. BR-27-94 for locations, notes and details.
- 5) Perform full and partial depth deck repairs to the existing concrete deck. See Dwg. No. BR-27-94 for notes and details.
- 6) Repair deteriorated ends of existing concrete beams in Spans 1-4 (total of 15 locations). See Dwg. No. BR-27-96 for locations, notes and details.
- 7) Perform concrete beam repairs to existing exterior beam in Span 3. See Dwg. No. BR-27-95 for location, notes and details.
- 8) Repair damaged area on existing concrete wing at Abutment 2. See Dwg. No. BR-27-94 for location, notes and details.
- 9) Clean and paint all existing steel bearing devices. See paint notes on Dwg. No. BR-27-93.

SPECIAL NOTE: See "Construction Sequence" on Dwg. No. BR-27-93.

LIST OF DRAWINGS

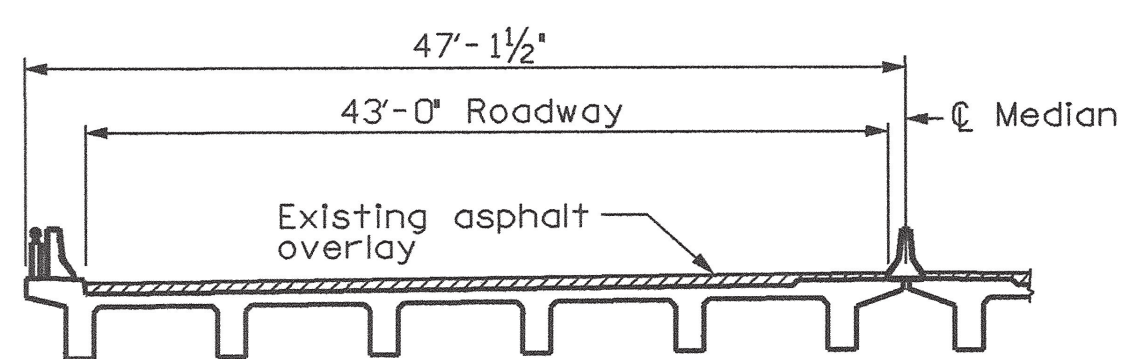
DWG. NO.	LAST REV. DATE	DRAWING
BR-27-92	2-26-98	LAYOUT OF BRIDGE TO BE REPAIRED (2)
BR-27-93	2-26-98	ESTIMATED QUANTITIES AND GENERAL NOTES
BR-27-94	2-26-98	BRIDGE REPAIR DETAILS (1)
BR-27-95	2-2-98	BRIDGE REPAIR DETAILS (2)
BR-27-96	2-2-98	CONCRETE BEAM END REPAIR DETAILS

LIST OF REFERENCE DRAWINGS

DWG. NO.	LAST REV. DATE	DRAWING
F-11-45 thru 49; F-11-13, 15, 28	Varies	Original Bridge Drawings (1957)
F-11-45A	Varies	Bridge Repair Drawings (1988)

LIST OF SPECIAL PROVISIONS

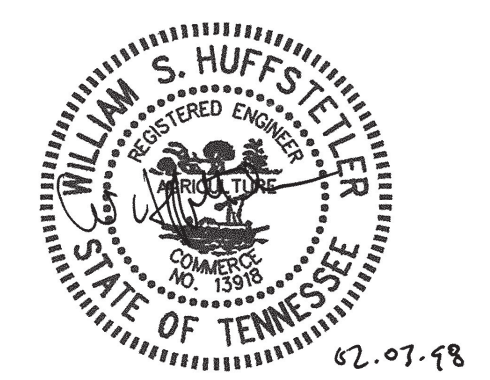
NO.	LAST REV. DATE	SPECIAL PROVISION
105A	03-06-95	Regarding Approval of Shop Drawings



TYPICAL SECTION

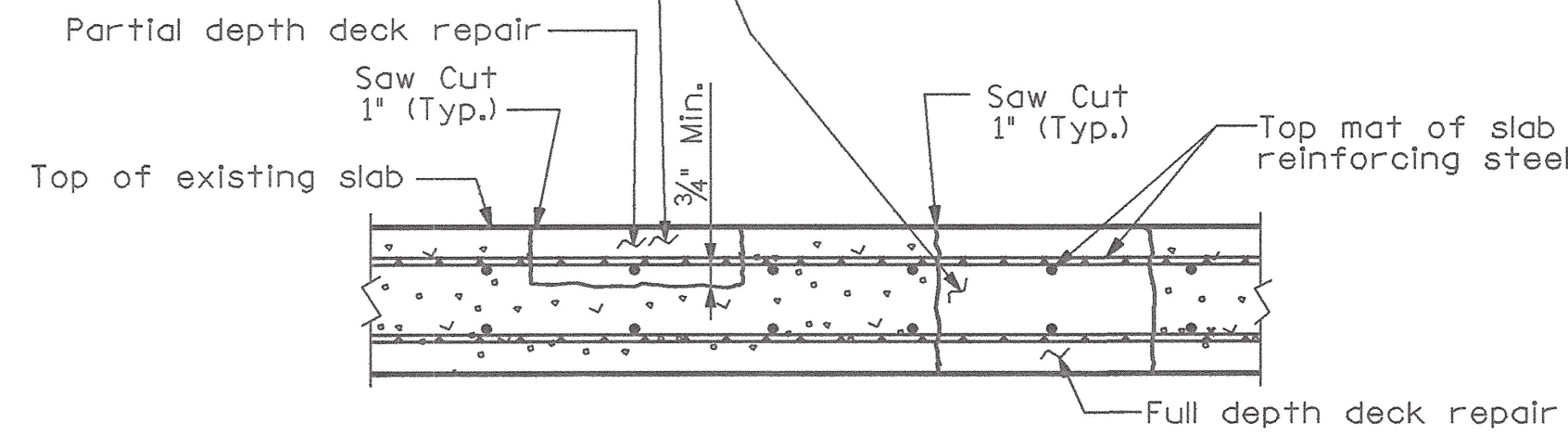
(Looking opposite direction of traffic)

DESIGNED BY	BLS	DATE	2-94
DRAWN BY	RMH	DATE	2-94
SUPERVISED BY	WSH	DATE	2-94
CHECKED BY	BLS	DATE	12-97



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
LAYOUT OF BRIDGE TO BE REPAIRED (2)
INTERSTATE I-275 OVER
OLDHAM AVE.
BRIDGE NO. 47-I275-1.02 LT.
KNOX COUNTY
1998

Concrete for deck repairs shall be High Early Strength Concrete, $f'c=3000$ p.s.i. @ 18 hour strength. See "Special Note to Contractor Concerning Concrete", on Dwg. No. BR-27-93.



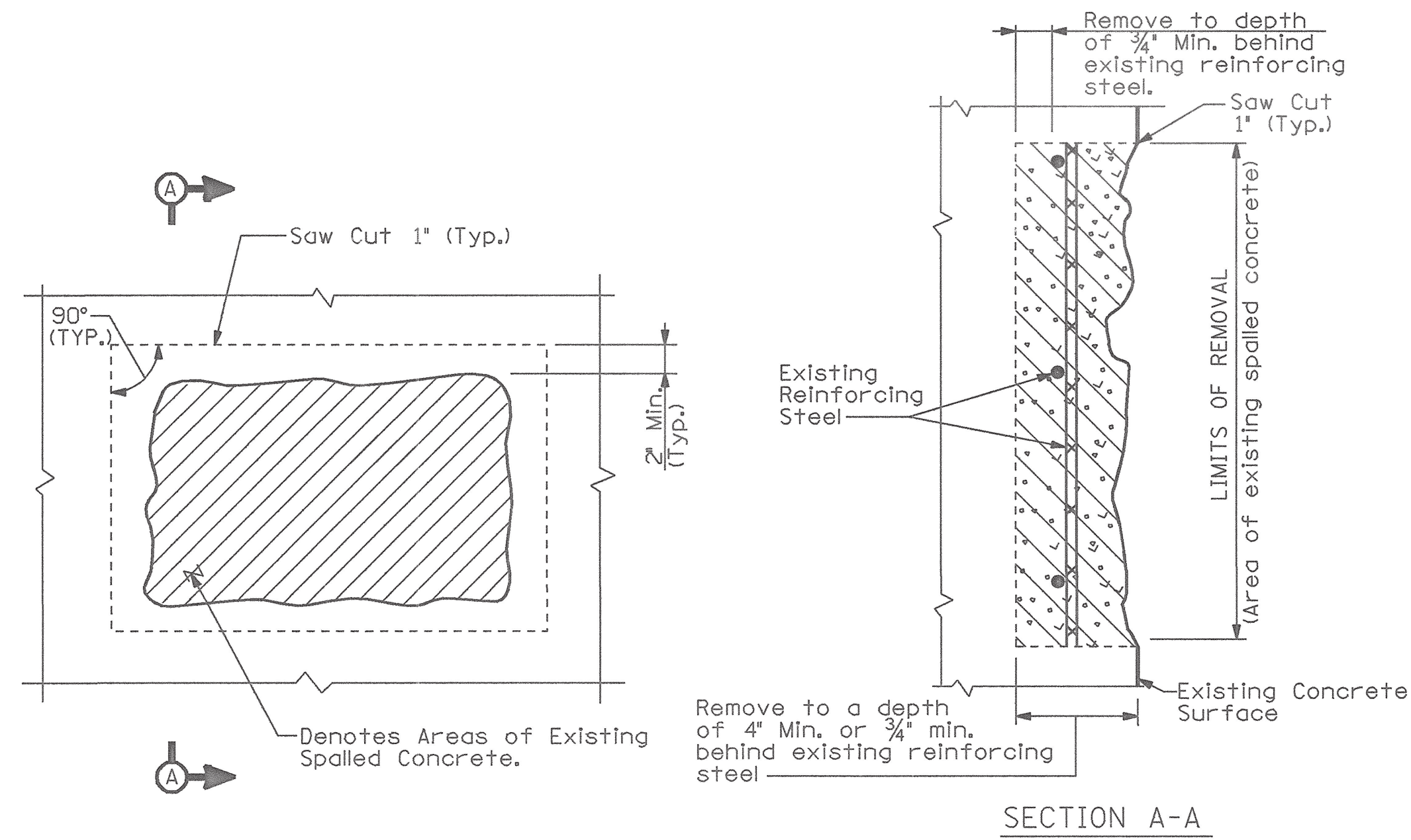
DETAIL SHOWING FULL AND PARTIAL DEPTH DECK REPAIR

Remove concrete in all delaminated areas to a depth of $\frac{3}{4}$ " below the top bar of the top mat of reinforcing steel. All reinforcing steel in areas of deck repair shall be completely cleaned. Areas of concrete removal shall be designated by personnel from the Bridge Repair Office. Inspections to determine areas of deck repair shall be scheduled with the Bridge Repair Office at least three (3) days in advance. Deck repair will be paid for under Item No. 604-10.50, Bridge Deck Repair (Partial Depth of Slab) and Item No. 604-10.30, Bridge Deck Repair (Full Depth of Slab). During partial depth repairs, should deteriorated concrete be encountered which appears to run full depth in the slab, the Engineer may designate these areas to be repaired under Item No. 604-10.30. Power driven hand tools used for the removal of unsound concrete in making partial and full depth repairs are subject to the following restrictions: 1) (Partial Depth Repairs) Pneumatic hammers heavier than nominal 60 pound class shall not be used. 2) (Full Depth Repairs) Pneumatic hammers heavier than nominal 90 pound class shall not be used. Also all deck repair over beams will be restricted to 60 pound pneumatic hammers. 3) Chipping hammers of the 15 pound class shall be used to remove concrete from beneath any reinforcing steel. 4) Traffic control shall be provided for traffic below bridge during partial and full depth deck repair.

Item No. 604-10.30 and 604-10.50 shall be bid with the contingency that these items may be increased, decreased or eliminated as directed by the Engineer.

SPECIAL NOTE:

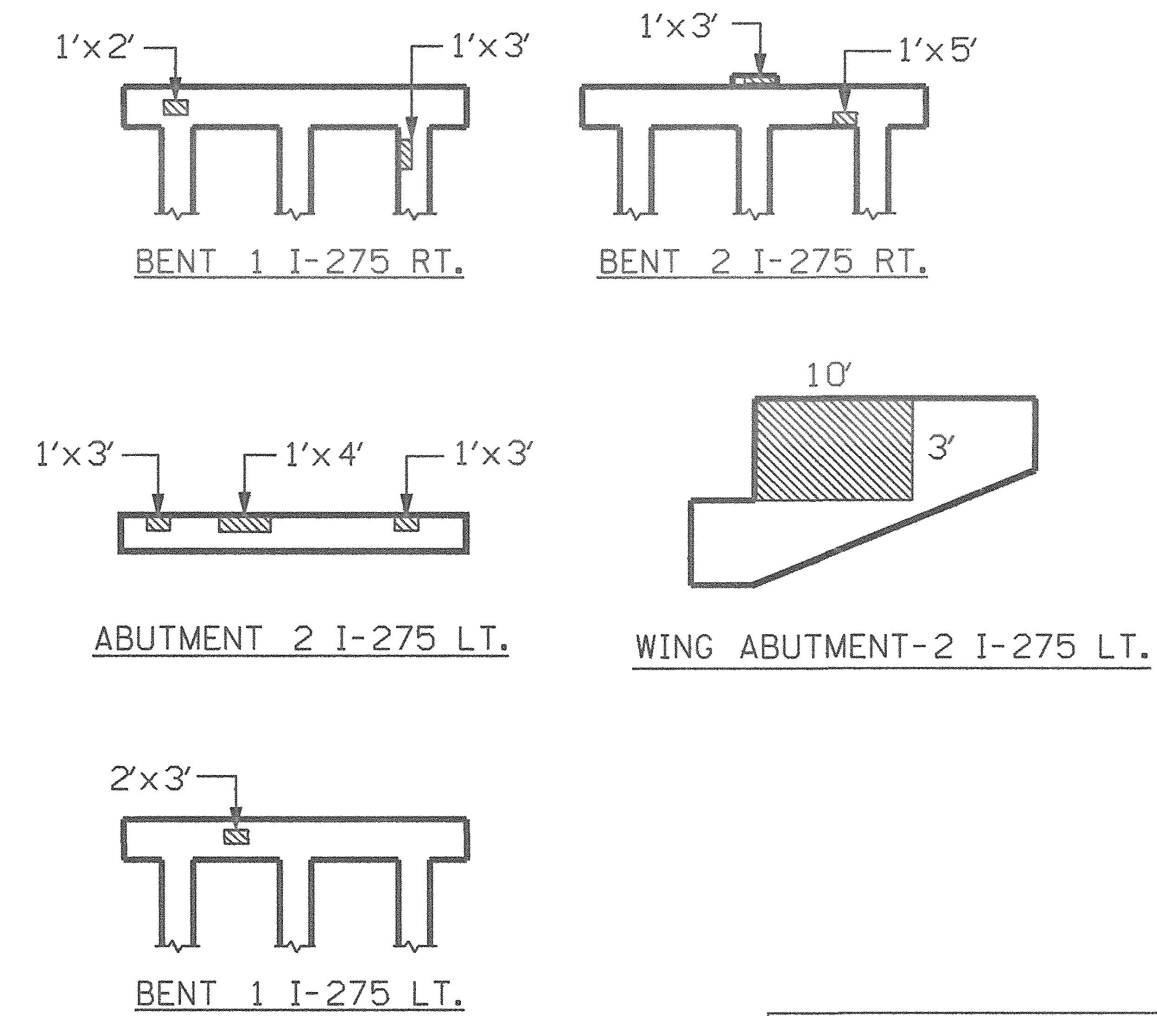
Deck repairs shall be performed in accordance with "Construction Sequence" shown on Dwg. No. BR-27-93.



DETAILS OF SPALLED CONCRETE SURFACE REMOVAL AND REPAIR

NOTES: Saw cut existing concrete surfaces so as to obtain a square area. Existing reinforcing steel shall be completely cleaned to remove all rust. Repair to original lines with high early strength concrete.

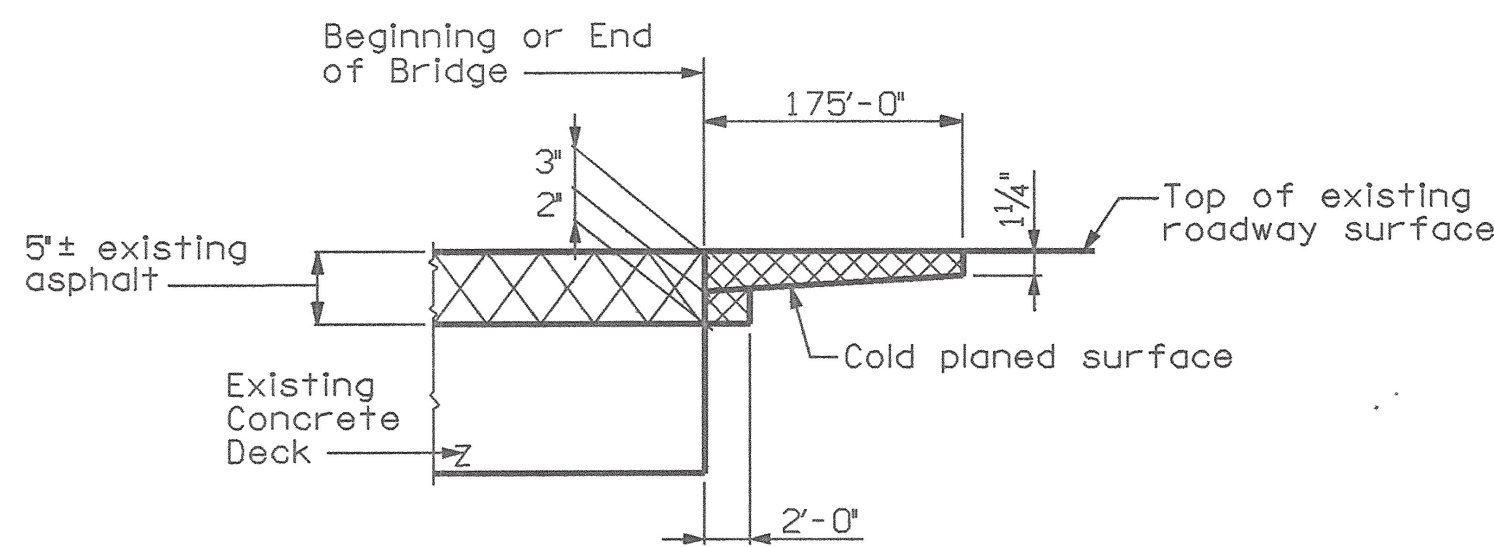
Cost of saw cutting, removing spalled concrete, completely cleaning exposed reinforcing steel, high early strength concrete, labor and any miscellaneous materials necessary to complete the repairs as shown shall be included in Item No. 604-10.54, Concrete Repairs, S.F.



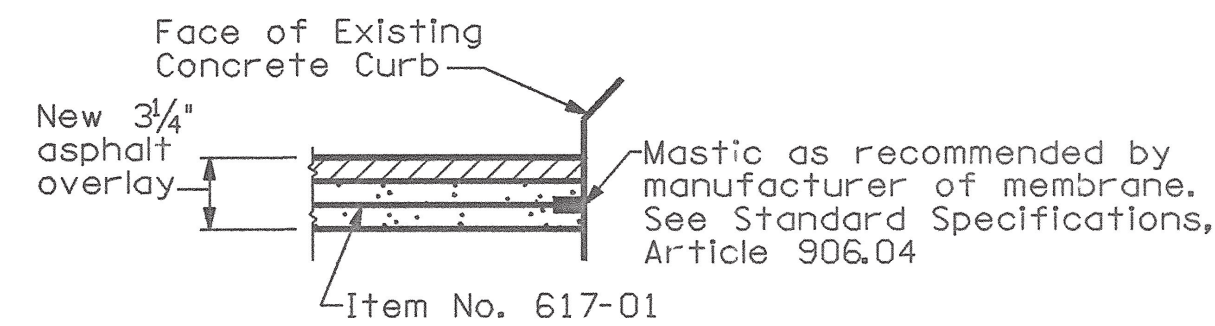
APPROX. SPALLED CONCRETE REPAIR AREAS			
Bent 1 I-275 Rt.	5 S.F.	Abutment 2 I-275 Lt.	10 S.F.
Bent 2 I-275 Rt.	10 S.F.	Wing Abutment 2 I-275 Lt.	30 S.F.
Bent 1 I-275 Lt.	10 S.F.		

NOTE: The Engineer shall designate all spalled concrete repair areas in the field. Quantities given are approximate. Item No. 604-10.54 shall be bid with the contingency that it may be increased, decreased or eliminated as directed by the Engineer.

▨ - Denotes area of spalled concrete to be removed and repaired.



ASPHALT REMOVAL



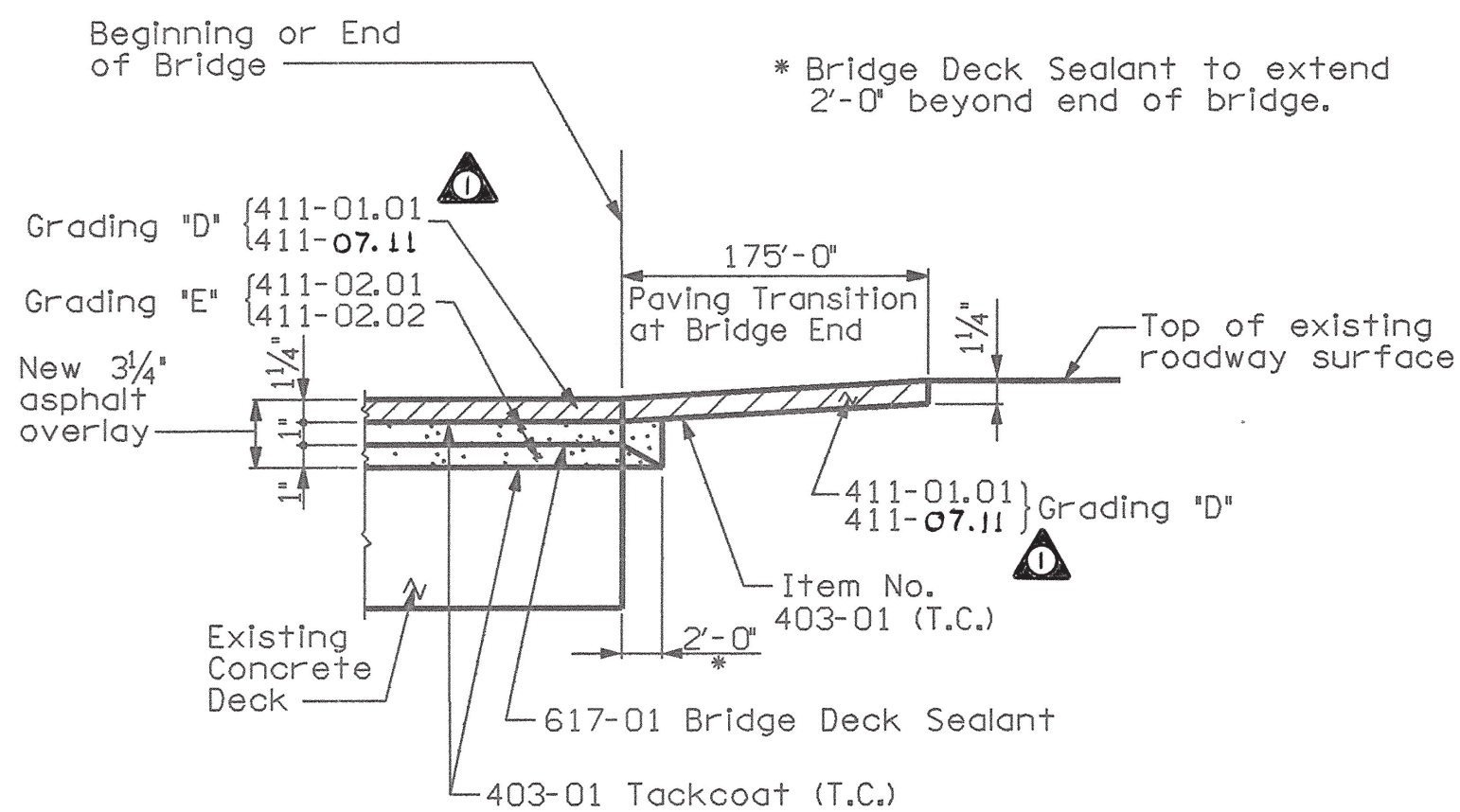
▨ - Denotes asphalt to be removed.

Removal of existing asphalt from bridge deck for full width and length of bridge to be paid for under Item No. 604-10.14.

Removal of existing asphalt from transitions for full width of approaches to be paid for under Item No. 415-01.02

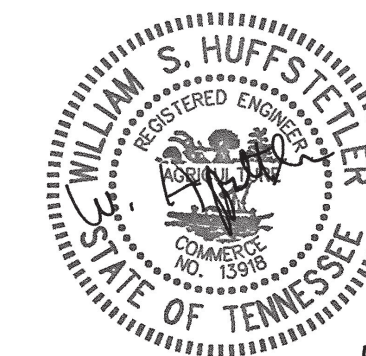
NOTE: When placing new $\frac{3}{4}$ " asphalt overlay with seal Contractors shall make provisions to maintain the existing bridge deck drain openings. These drains are to be cleaned and remain open. Cost to be included in Items bid on.

See Sections 617 and 906.04 of the Tennessee Standard Specifications for Road and Bridge Construction.



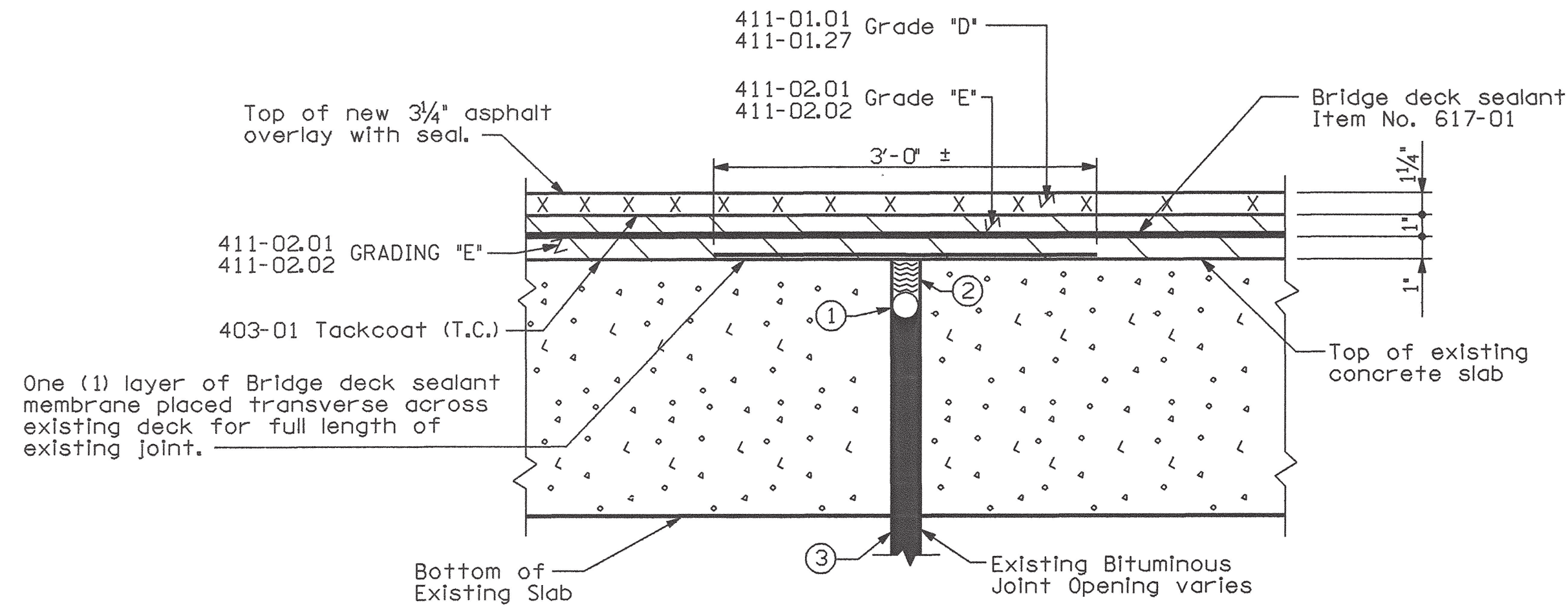
BRIDGE DECK SEALANT AND PAVEMENT TRANSITION DETAIL
No Scale

DESIGNED BY: BLS DATE: 7-94
 DRAWN BY: PGR DATE: 7-94
 SUPERVISED BY: WSH DATE: 7-94
 CHECKED BY: BLS DATE: 12-97



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE REPAIR DETAILS (1)
 INTERSTATE I-275 OVER
 OLDHAM AVE.
 BRIDGE NO. 47-I275-1.02 RT. & LT.
 KNOX COUNTY
 1998

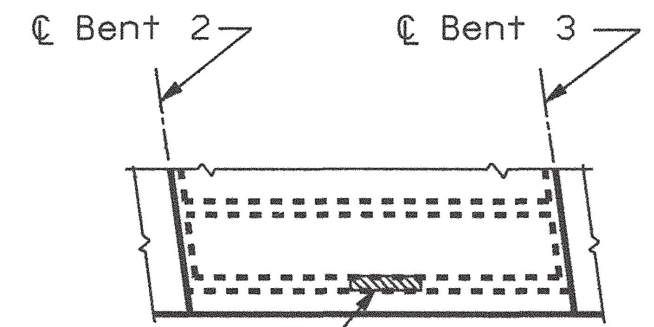
EST. TN10568F.DCN 03 FEB 1998 14:05



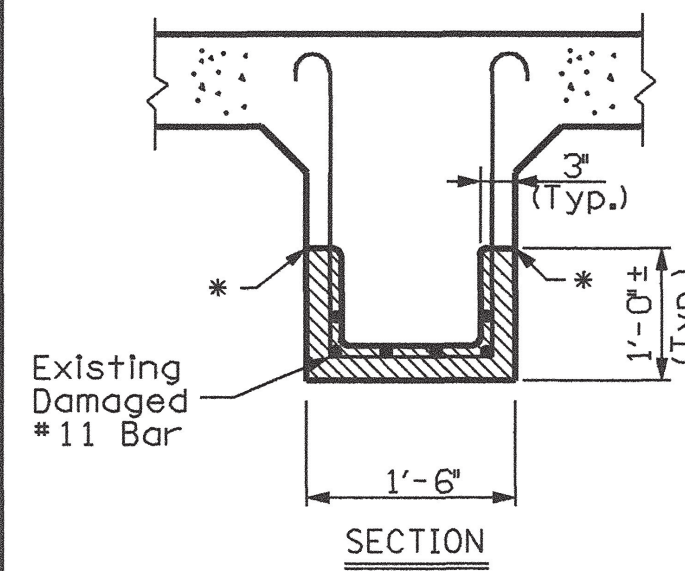
EXISTING BITUMINOUS JOINT REPAIR DETAIL

(Bents 1-3, I275 RT. & LT.)
(Total of six (6) locations)

- ① NOTE: The movement gap shall be caulked with a backer rod of suitable diameter. The rod shall be placed at a depth to ensure the correct width/depth ratio of the new bridge joint sealer. Backer rod and caulk shall be as per the sealer manufacturer.
 - ② NOTE: Top 2" of all existing bituminous joints in the bridge slab to be cleaned and resealed with new joint sealer. Contractor has the option of using either:
 - 1) A Type II hot-poured elastic type concrete joint sealer. See Standard Specifications Section 905.05, Joint Sealers.
 - 2) A cold pour single component joint sealer as approved by the Division of Materials and Test.
- NOTE: Cost of joint cleaning and sealing to be included under Item No. 617-01 (Bridge Deck Sealant). The sealer shall extend up and across the curb or sidewalk area to the outer edge of the bridge slab.
- ③ All existing bituminous joint material and debris shall be removed for full length and depth of joint.
- Joints shall be cleaned to full satisfaction of the Engineer before it is sealed as shown in above detail.
- All cost of removing joint material and debris and for cleaning joint surfaces shall be included in Item No. 617-01.



PART PLAN I-275 LT.



CONCRETE BEAM REPAIR-DETAIL A

* Saw Cut 1' (Typ.)

Ⓐ 8'-0"± Beam Repair, see Repair Detail A

▨ - Denotes limits of concrete removal and areas to be repoured.

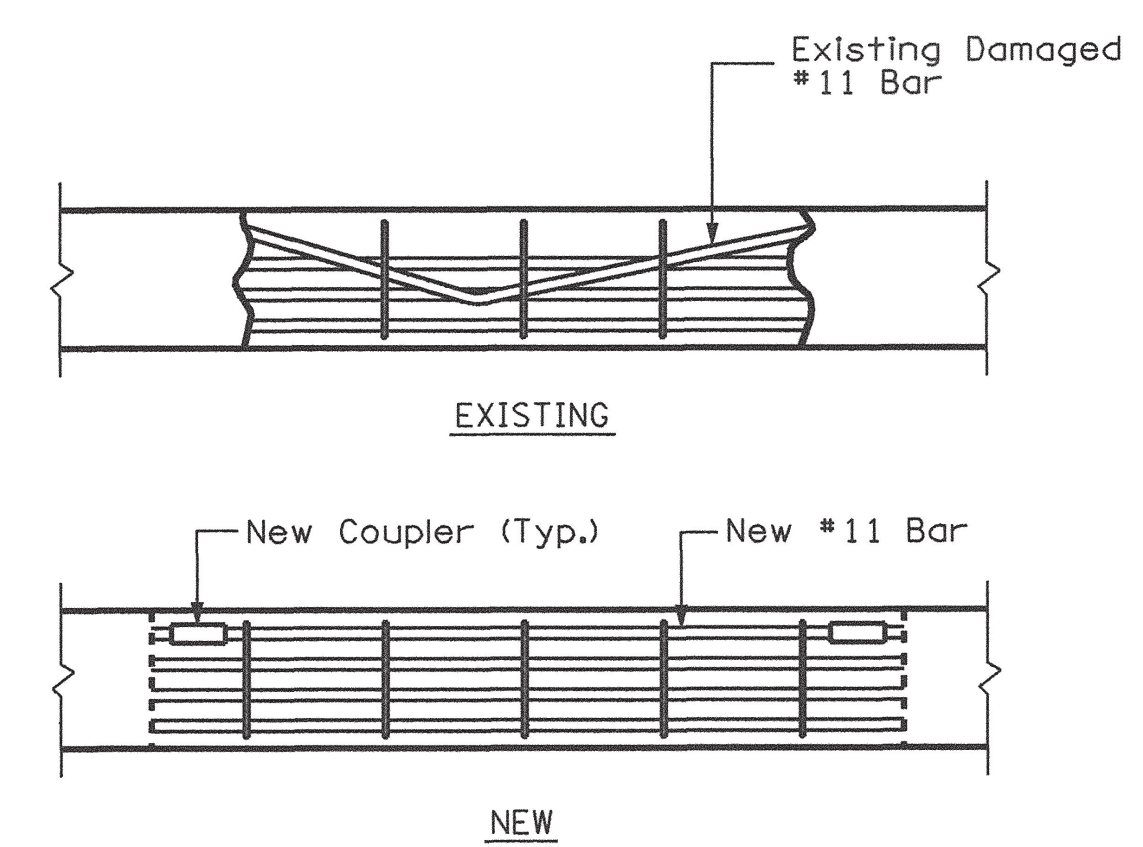
CONSTRUCTION PROCEDURE FOR REPAIRING DETERIORATED AREAS ON CONCRETE BEAMS

1. Saw cut and remove portions of concrete beams as shown in these plans to insure that sound concrete is reached in all areas.
2. Completely clean all exposed reinforcing steel to the full satisfaction of the Engineer.
3. Repair damaged reinforcing steel as detailed below.
4. Repair beam to original lines with high early strength concrete, f'c = 3000 psi at 18 hour strength. See 'Special Note to Contractor Concerning Concrete' on Dwg. No. BR-27-93.
- Ⓐ 5. Beam repair shall be performed during Phase I construction for I-275 Lt. See 'Construction Sequence' on Dwg. No. BR-27-93.

NOTE: Contractor shall take extreme care when removing existing concrete so as not to damage the existing reinforcing. Any damage shall be repaired, at the Contractor's expense, to the full approval of the Engineer.

NOTE: Cost of saw cutting, removing damaged concrete, cleaning reinforcing, new reinforcing steel and couplers, high early strength concrete, labor and any miscellaneous materials necessary for completing repairs to the damaged concrete beam, as directed by the Engineer, shall be paid for under Item No. 604-10.54, Concrete Repairs, S.F.

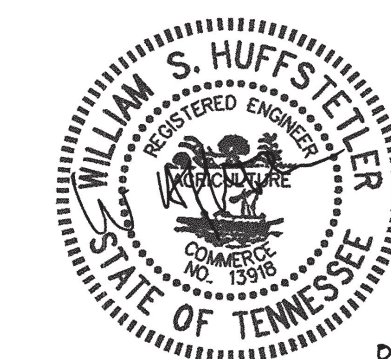
SPECIAL NOTE TO CONTRACTOR: Concrete beam repair shown on this sheet are approximate areas and lengths only, and may be increased or decreased as directed by the Engineer. Additional locations within the bridge may be designated to receive similar type repairs. All areas of beam repair shall be designated by the Engineer. Any additional areas shall be paid for at the unit price bid for Item No. 604-10.54, Concrete Repairs, S.F. (Approx. beam repair area = 25 S.F.)



REINFORCING REPAIR

1. Remove sufficient amount of concrete from beam to expose enough straight reinforcing to attach coupler.
2. Remove existing #11 bar by cutting. Flame cutting will not be allowed.
3. Attach new #11 bar using BAR-LOCK MBT couplers. Length of new #11 bar to be determined in field. Couplers can be obtained from:

BAR-LOCK MBT Couplers
P.O. Box 28280
Bellingham, Washington 98228
800-755-4888
360-798-1891



02.01.99

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE REPAIR DETAILS (2)
INTERSTATE I-275 OVER
OLDHAM AVE.
BRIDGE NO. 47-I275-1.02 RT. & LT.
KNOX COUNTY
1998

DESIGNED BY	BLS	DATE	7-94
DRAWN BY	PCR	DATE	7-94
SUPERVISED BY	WSH	DATE	7-94
CHECKED BY	BLS	DATE	12-97

